

TOWN OF POUGHKEEPSIE DEPARTMENTAL SIGNATURES  
(Required Before Chairman's Signature)

|                        |      |
|------------------------|------|
| Planning Department    | Date |
| Building Department    | Date |
| Fire Department        | Date |
| Water Department       | Date |
| Sewer Department       | Date |
| Zoning Administrator   | Date |
| Engineering Department | Date |



**WESTCHESTER MEDICAL CENTER**  
WORLD-CLASS MEDICINE THAT'S NOT A WORLD AWAY.

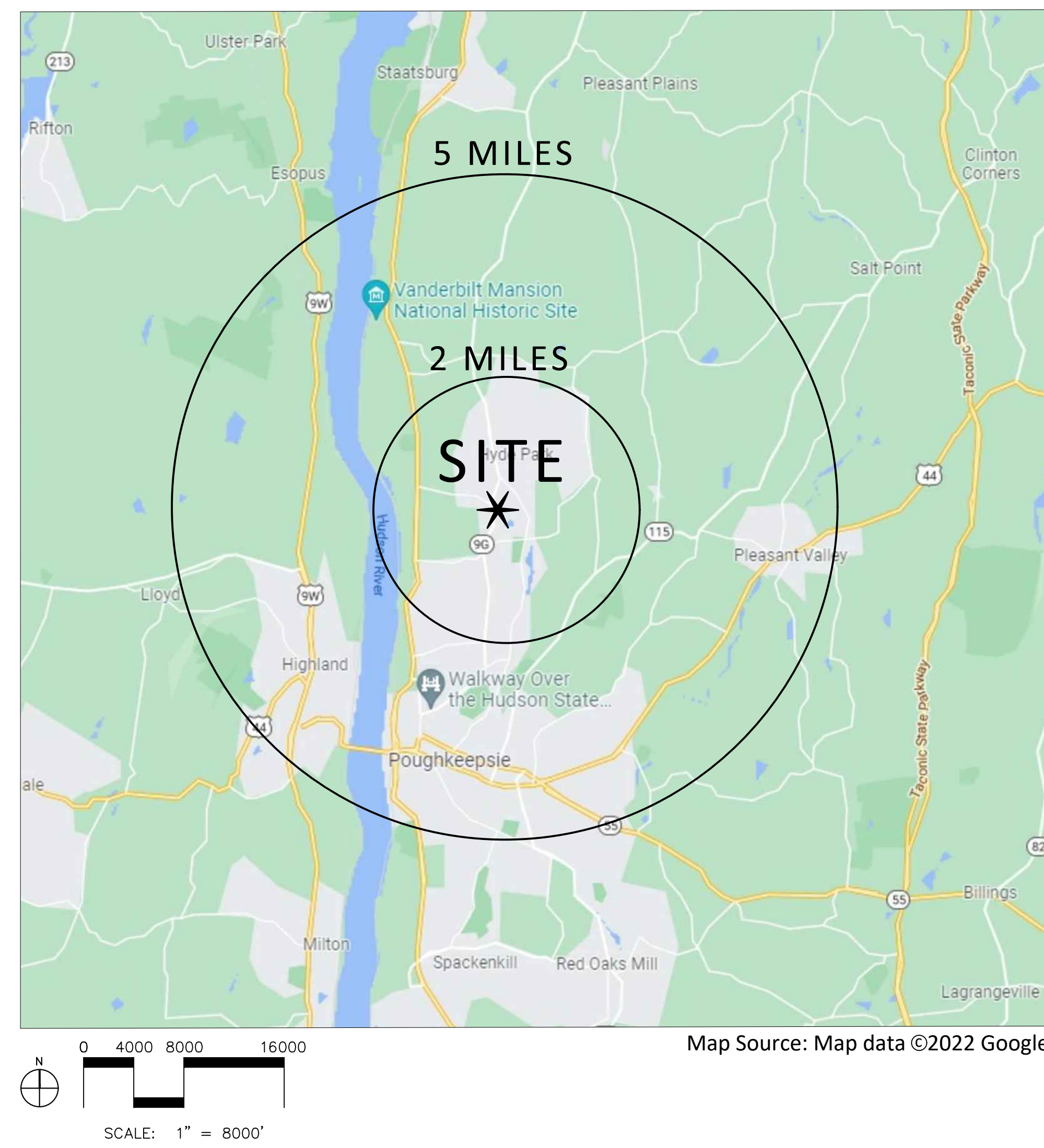
**MID-HUDSON REGIONAL HOSPITAL**  
Poughkeepsie, New York  
**PAVEMENT REPAIRS AT BAKER AVENUE AND HELIPAD**  
**CONTRACT DOCUMENT SUBMISSION**  
**WMC CONTRACT NUMBER WMC-3829**  
**OGS PROJECT NO. SE391**  
**PERMIT SET - 09/06/23**

TOWN OF POUGHKEEPSIE PLANNING BOARD  
PLAN APPROVAL

The plan of development for the property as depicted hereon was approved by a majority of the members of the Town of Poughkeepsie Planning Board at a meeting held on \_\_\_\_\_, and the conditions of Site Plan Approval have been satisfied or arrangements have been made to ensure the completion of any outstanding or incomplete conditions.

Chairman \_\_\_\_\_  
Date \_\_\_\_\_

SITE LOCATION MAP



WORK AREA AERIAL



LIST OF DRAWINGS

| No.     | Title                                                                                                                        | Date     | Scale  |
|---------|------------------------------------------------------------------------------------------------------------------------------|----------|--------|
|         | Cover Sheet                                                                                                                  |          |        |
|         | Survey                                                                                                                       | 03/03/22 | 1"=20' |
| C-101   | Layout Plan                                                                                                                  | 09/06/23 | 1"=20' |
| C-102   | Layout Plan                                                                                                                  | 09/06/23 | 1"=20' |
| C-201   | Construction Plan                                                                                                            | 09/06/23 | 1"=20' |
| C-202   | Construction Plan                                                                                                            | 09/06/23 | 1"=20' |
| C-301   | Removals Plan                                                                                                                | 09/06/23 | 1"=20' |
| C-302   | Removals Plan                                                                                                                | 09/06/23 | 1"=20' |
| C-401   | Marking and Signage Plan                                                                                                     | 09/06/23 | 1"=20' |
| C-402   | Marking and Signage Plan                                                                                                     | 09/06/23 | 1"=20' |
| C-501   | Erosion Control Plan                                                                                                         | 09/06/23 | 1"=20' |
| C-502   | Erosion Control Plan                                                                                                         | 09/06/23 | 1"=20' |
| C-601   | Details                                                                                                                      | 09/06/23 | N/A    |
| C-602   | Details                                                                                                                      | 09/06/23 | N/A    |
| C-701   | General Notes and Tables                                                                                                     | 09/06/23 | N/A    |
| C-801   | Temporary Helipad Site Layout Plan                                                                                           | 09/06/23 | 1"=20' |
| CT-101  | Traffic Control Plan                                                                                                         | 09/06/23 | 1"=50' |
| E-001   | Electrical Symbols, Abv. and General Notes                                                                                   | 09/06/23 | N/A    |
| E-002   | Electrical Specs.                                                                                                            | 09/06/23 | N/A    |
| E-003   | Electrical Specs Cont.                                                                                                       | 09/06/23 | N/A    |
| E-101   | Electrical Site Plan - Demolition                                                                                            | 09/06/23 | 1"=30' |
| E-102   | Electrical Site Plan                                                                                                         | 09/06/23 | 1"=30' |
| E-103   | Electrical Site Plan - Light Photometrics                                                                                    | 09/06/23 | 1"=30' |
| E-701   | Electrical Details                                                                                                           | 09/06/23 | N/A    |
| 608-1   | Pedestrian Facilities, Sheets 1-12                                                                                           | 08/18/21 | N/A    |
| 609.01  | Concrete and Stone Curb and Gutter, Sheets 1-4                                                                               | 08/04/21 | N/A    |
| 619-010 | Work Zone Traffic Control General Notes, Sheets 1-2                                                                          | 12/21/22 | N/A    |
| 619-011 | Work Zone Traffic Control General Tables and Legend, Sheets 1-2                                                              | 12/21/22 | N/A    |
| 619-322 | Work Zone Traffic Control Two-Lane Two Way roadway Crosswalk Closure and Pedestrian Detour Short Term Operations, Sheets 1-2 | 04/08/22 | N/A    |
| 685-01  | Pavement Marking Details, Sheets 1-9                                                                                         | 09/27/22 | N/A    |

NOTE: SEE SHEET C-701 FOR NYSDOT STANDARD SHEET NUMBER AND TITLE

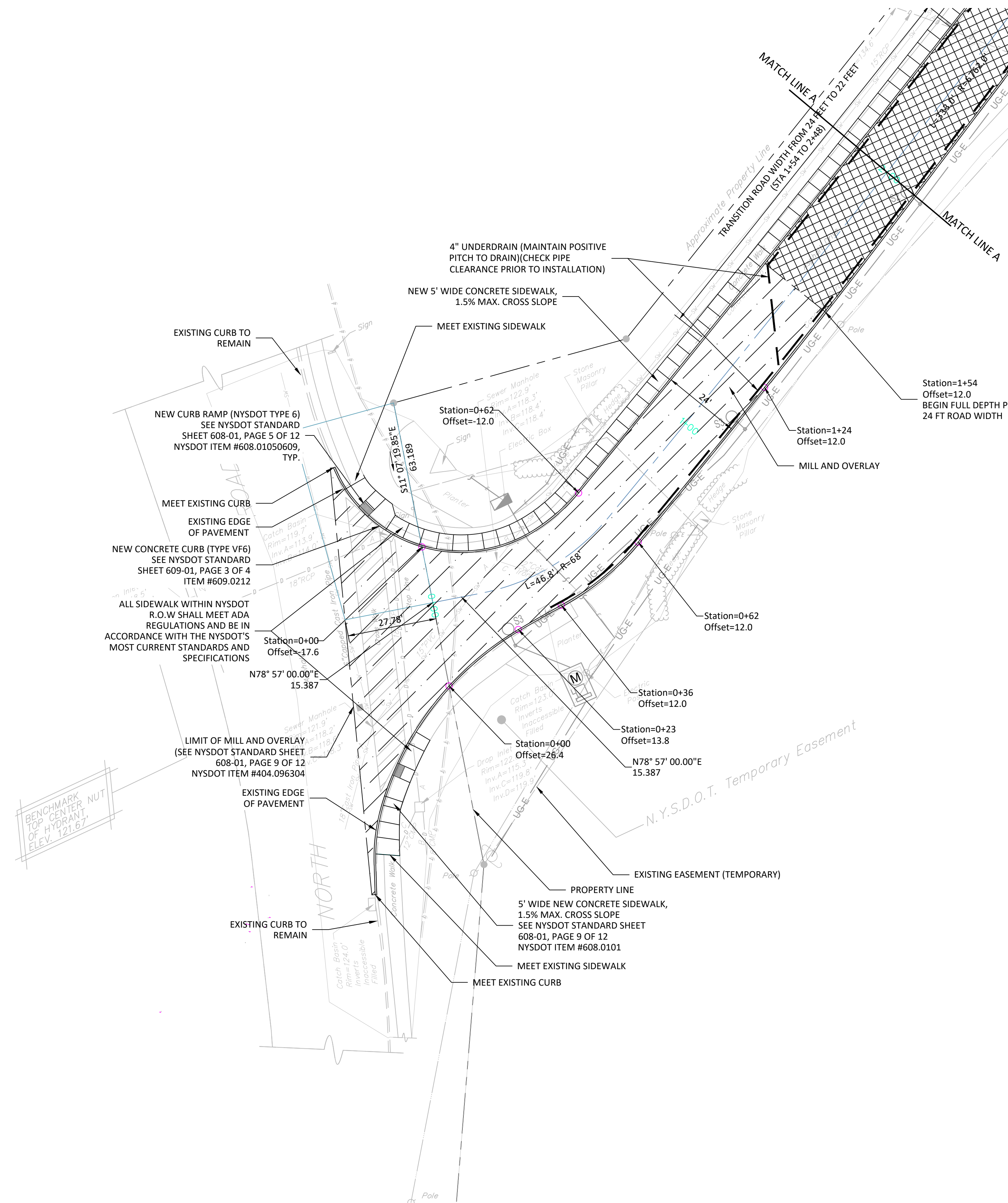
**APPLICANT/ PROPERTY OWNER:**  
WESTCHESTER COUNTY  
HEALTH CARE CORPORATION  
100 Woods Road  
Valhalla, NY 10595

**CIVIL / TRAFFIC ENGINEER & LANDSCAPE ARCHITECT:**  
**DTS • PROVIDENT**  
Intelligent Land Use  
DTS Provident Design Engineering, LLP  
One North Broadway  
White Plains, NY 10601  
P: 914.428.0010  
F: 914.428.0017  
CA#0017846

**SURVEYOR:**  
BADEY & WATSON  
3063 Route 9  
Cold Spring, NY 10516

UNIFORM CODE STATEMENT

TO THE BEST OF THE REGISTERED DESIGN PROFESSIONAL'S KNOWLEDGE, BELIEF AND PROFESSIONAL JUDGEMENT, THESE PLANS AND/OR SPECIFICATIONS ARE IN COMPLIANCE WITH THE 2020 UNIFORM CODE.



| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS   | 06/22/2023 |
| 3   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 4   | PERMIT SET                    | 09/06/2023 |
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**LAYOUT LEGEND**

- PROPERTY LINE
- - - ROAD CENTERLINE
- == PROPOSED CURB
- == PROPOSED DROP CURB
- [Pattern] PROPOSED CONCRETE
- [Pattern] PROPOSED MILL AND OVERLAY
- [Pattern] NYS DOT PAVEMENT SECTION
- [Pattern] PROPOSED ASPHALT
- [Pattern] PROPOSED CONCRETE PAD
- [Pattern] PROPOSED HELIPAD



**MID-HUDSON REGIONAL HOSPITAL**  
POUGHKEEPSIE, NEW YORK



**Westchester**  
MEDICAL CENTER  
WESTCHESTER COUNTY  
HEALTH CARE CORPORATION  
100 Woods Road  
Valhalla, NY 10595

APPLICANT

**DTS • PROVIDENT**  
Intelligent Land Use

DTS Provident Design Engineering, LLP  
One North Broadway  
White Plains, NY 10601  
P: 914.428.0010 CA#: 0017846  
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**Mid-Hudson Regional Hospital**  
241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

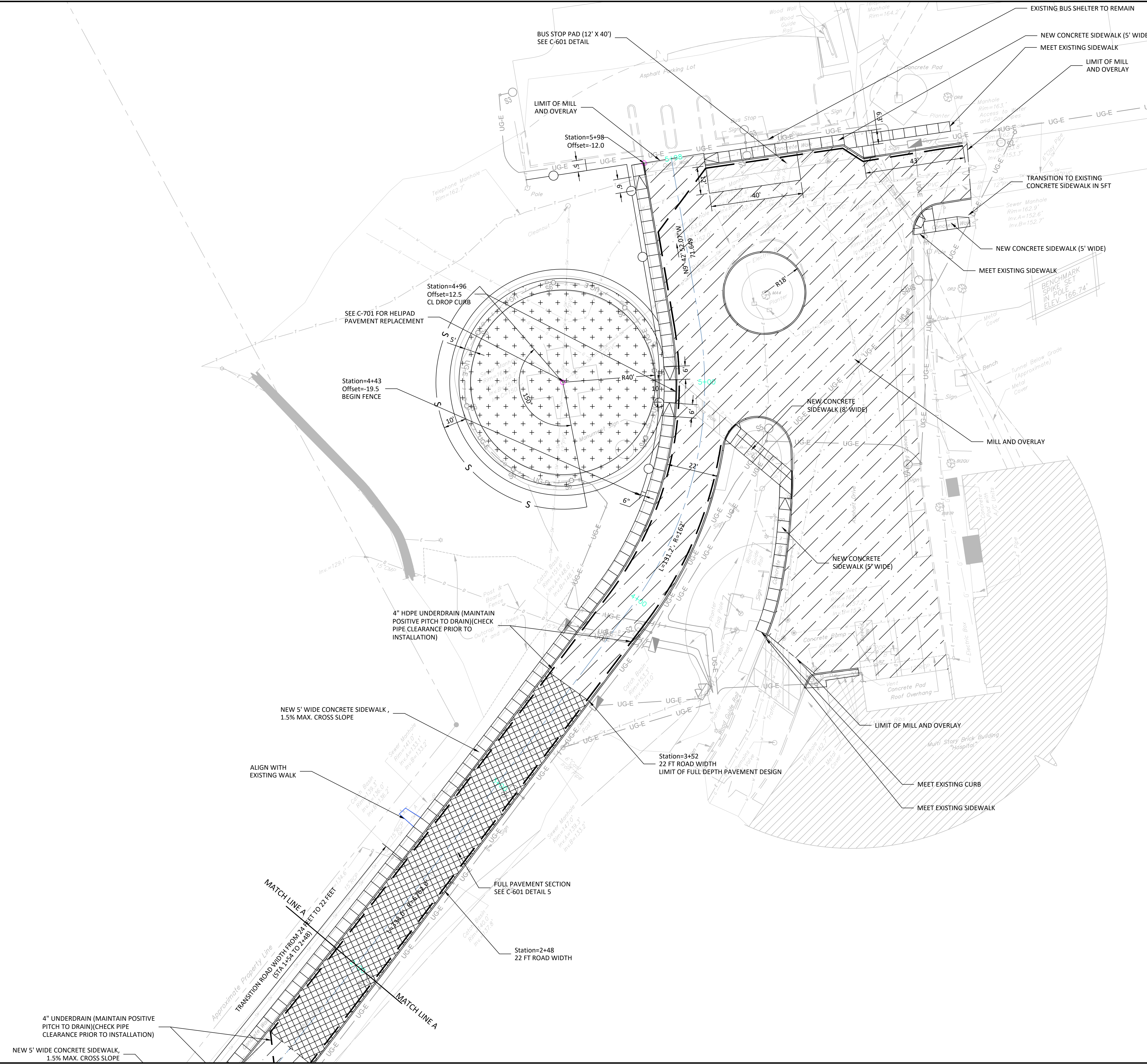
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**LAYOUT PLAN**

|              |          |
|--------------|----------|
| Scale:       | 1"=20'   |
| Date:        | 07/27/22 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 1 of 22  |
| Dwg. No.:    | C-101    |

EXPIRES 1/31/24



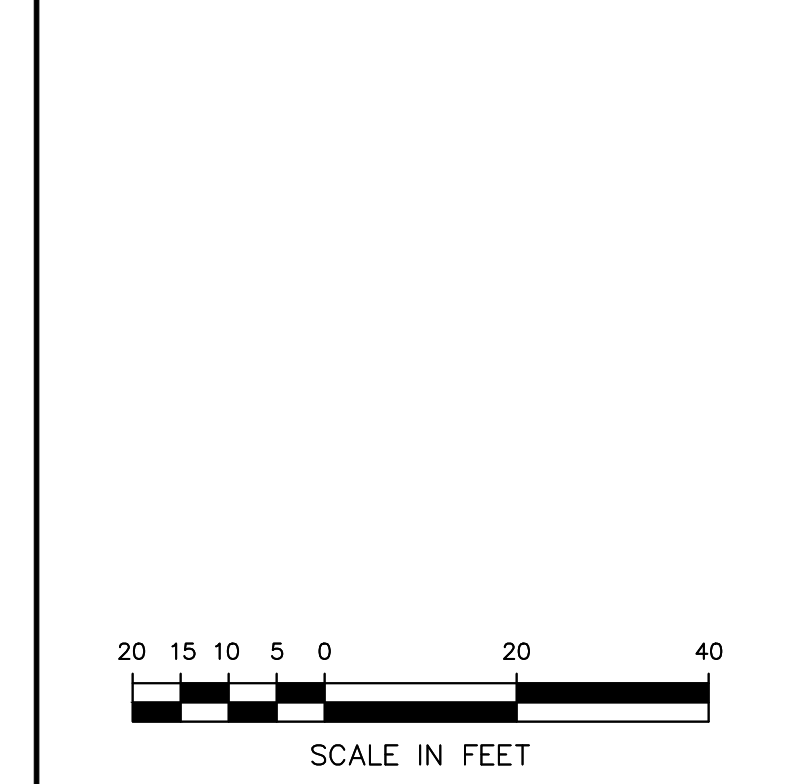
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| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS   | 06/22/2023 |
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| 4   | PERMIT SET                    | 09/06/2023 |
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**LAYOUT LEGEND**

- PROPERTY LINE
- ROAD CENTERLINE
- PROPOSED CURB
- PROPOSED DROP CURB
- [Pattern: Diagonal lines /] PROPOSED CONCRETE
- [Pattern: Diagonal lines \] PROPOSED MILL AND OVERLAY
- [Pattern: Horizontal lines] NYS DOT PAVEMENT SECTION
- [Pattern: Vertical lines] PROPOSED ASPHALT
- [Pattern: Dashed lines] PROPOSED CONCRETE PAD
- [Pattern: Plus signs] PROPOSED HELIPAD



**MID-HUDSON  
REGIONAL HOSPITAL  
POUGHKEEPSIE, NEW YORK**

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100 Woods Road  
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Mid-Hudson Regional Hospital  
241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

TITLE:  
**LAYOUT PLAN**

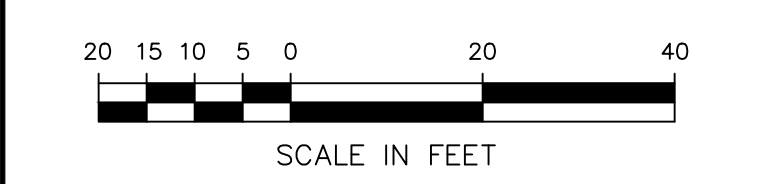
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| Drawn By:    | DC           |
| Checked By:  | GMS          |
| Project No.: | 0878         |
| Sheet No.:   | 2 of 22      |
| Dwg. No.:    | <b>C-102</b> |

EXPIRES 1/31/24

| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | REVISIONS PER OGS COMMENTS    | 03/08/2023 |
| 2   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 3   | REVISIONS PER TOWN COMMENTS   | 06/22/2023 |
| 4   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 5   | PERMIT SET                    | 09/06/2023 |

**CONSTRUCTION PLAN LEGEND**

- PROPOSED PROPERTY LINE
- PROPOSED EASEMENT LINE
- PROPOSED ROAD BASELINE
- EXISTING CURB
- PROPOSED CONC. CURB
- PROPOSED EDGE OF PAVEMENT
- [Pattern] PROPOSED CONCRETE
- [Pattern] PROPOSED MILL AND OVERLAY
- [Pattern] NYSDOT PAVEMENT SECTION
- [Pattern] PROPOSED ASPHALT
- [Pattern] PROPOSED CONCRETE PAD
- [Pattern] PROPOSED HELIPAD
- PROPOSED DROP CURB
- LIMIT OF DISTURBANCE
- 210 PROPOSED CONTOUR LINE
- x 210.5 PROPOSED SPOT ELEVATION



**MID-HUDSON REGIONAL HOSPITAL**  
POUGHKEEPSIE, NEW YORK



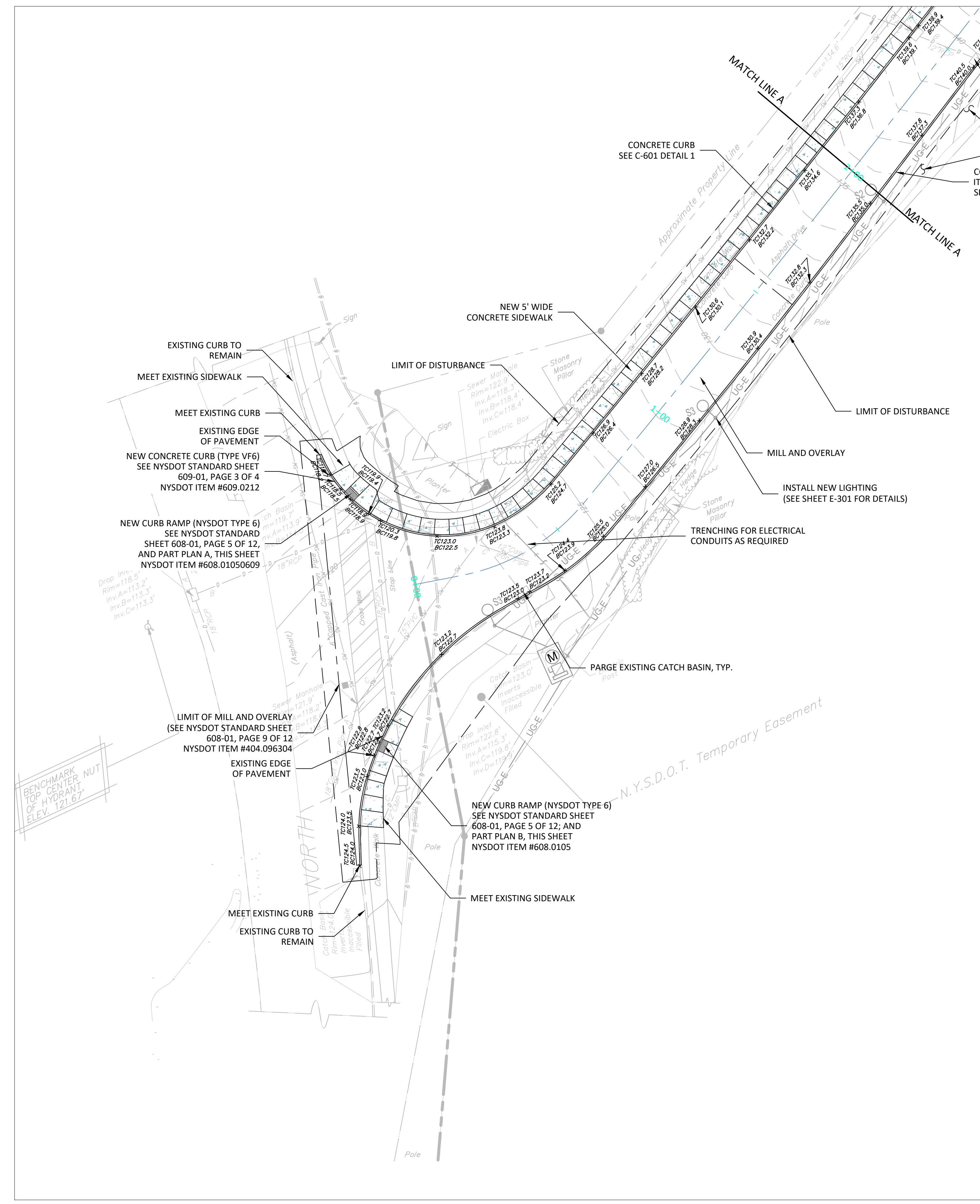
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One North Broadway  
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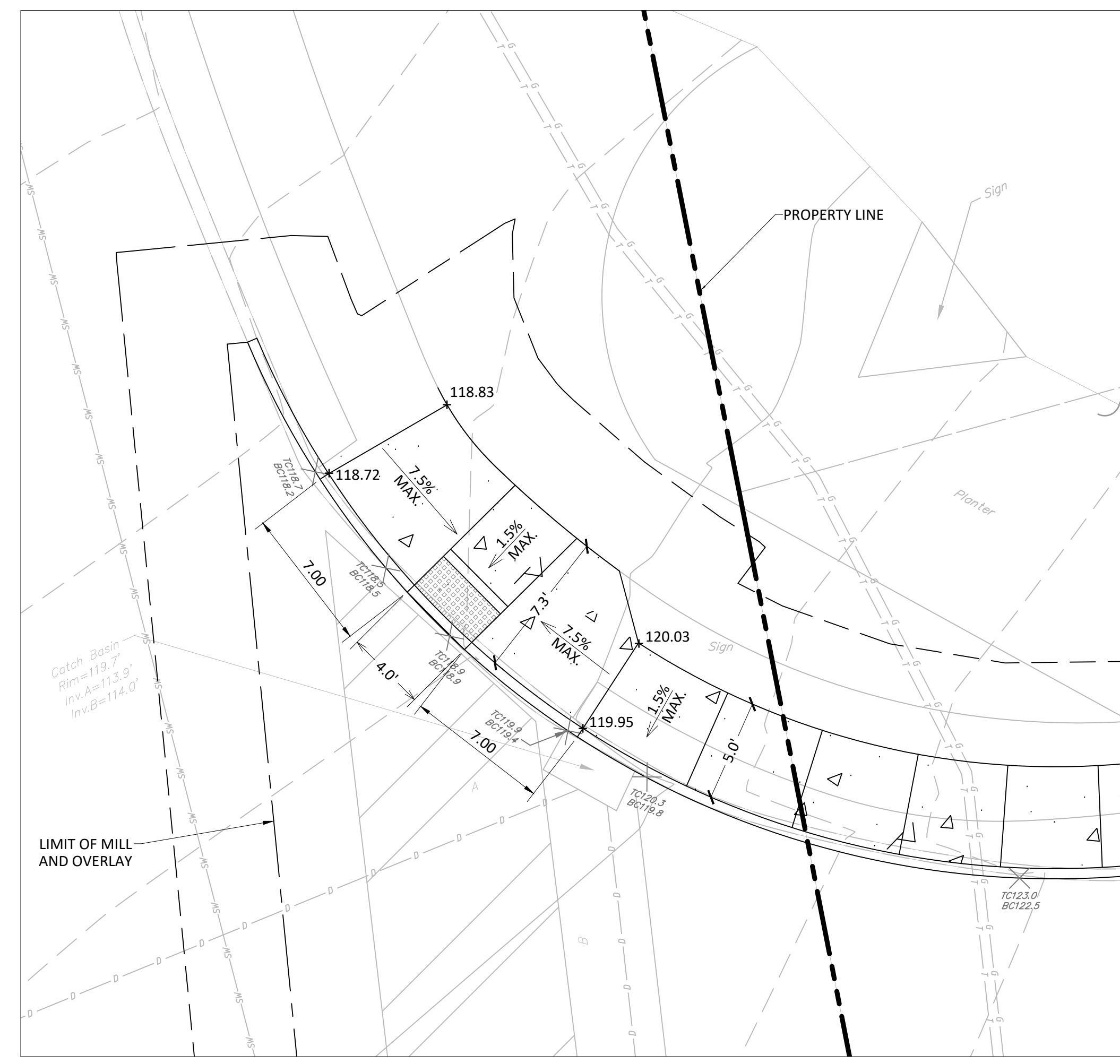
**Mid-Hudson Regional Hospital**  
241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

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| TITLE:       | CONSTRUCTION PLAN |
| Scale:       | 1"=20'            |
| Date:        | 07/27/22          |
| Drawn By:    | DC                |
| Checked By:  | GMS               |
| Project No.: | 0878              |
| Sheet No.:   | 3 of 22           |
| Dwg. No.:    | C-201             |

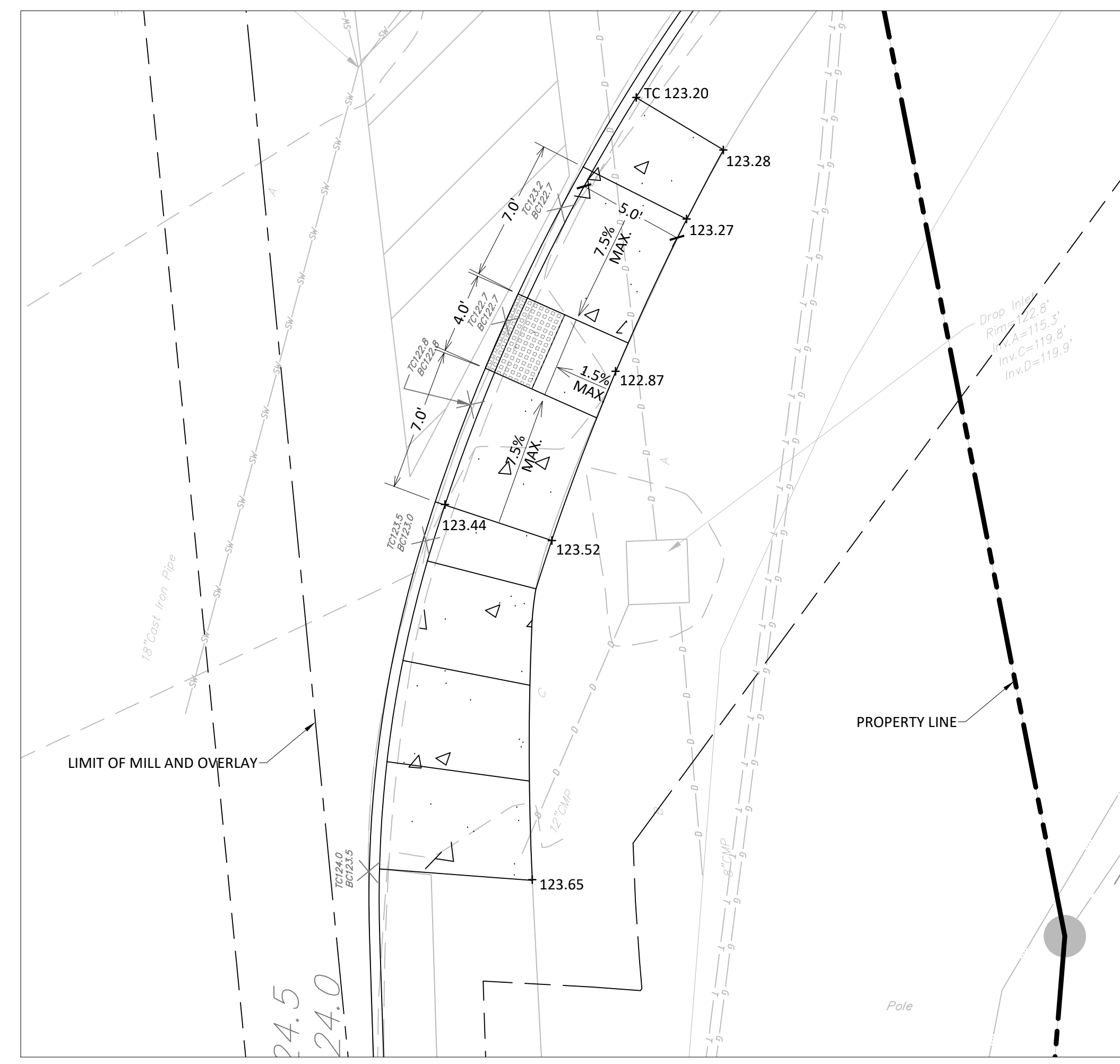
EXPIRES 1/31/24



- NOTES:**
- EXISTING MANHOLE COVERS, FRAME AND GRATES TO BE ADJUSTED TO FINISHED GRADE.
  - MAINTAIN EXISTING GRADES AND POSITIVE PITCH FOR DRAINAGE.
  - PROPOSED ELECTRIC TRENCH PAVEMENT SHALL BE FULL DEPTH PAVEMENT PER DETAIL 5 ON DRAWING C-601



**PART PLAN A**  
SCALE: 1" = 5'



**PART PLAN B**  
SCALE: 1" = 5'

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| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | REVISIONS PER OGS COMMENTS    | 03/08/2023 |
| 2   | SITE PLAN SUBMISSION          | 04/24/2023 |
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| 4   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 5   | PERMIT SET                    | 09/06/2023 |

| CONSTRUCTION PLAN LEGEND |                                 |
|--------------------------|---------------------------------|
|                          | PROPOSED PROPERTY LINE          |
|                          | PROPOSED EASEMENT LINE          |
|                          | PROPOSED ROAD BASELINE          |
|                          | EXISTING CURB                   |
|                          | PROPOSED CONC. CURB             |
|                          | PROPOSED EDGE OF PAVEMENT       |
|                          | PROPOSED CONCRETE               |
|                          | PROPOSED MILL AND OVERLAY       |
|                          | NYS DOT PAVEMENT SECTION        |
|                          | PROPOSED ASPHALT                |
|                          | PROPOSED CONCRETE PAD           |
|                          | PROPOSED HELIPAD                |
|                          | PROPOSED DROP CURB              |
|                          | LIMIT OF DISTURBANCE            |
|                          | PROPOSED CONTOUR LINE           |
|                          | x 210.5 PROPOSED SPOT ELEVATION |



**MID-HUDSON REGIONAL HOSPITAL**  
POUGHKEEPSIE, NEW YORK



WESTCHESTER COUNTY HEALTH CARE CORPORATION  
100 Woods Road  
Valhalla, NY 10595

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Mid-Hudson Regional Hospital  
241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

TITLE: **CONSTRUCTION PLAN**

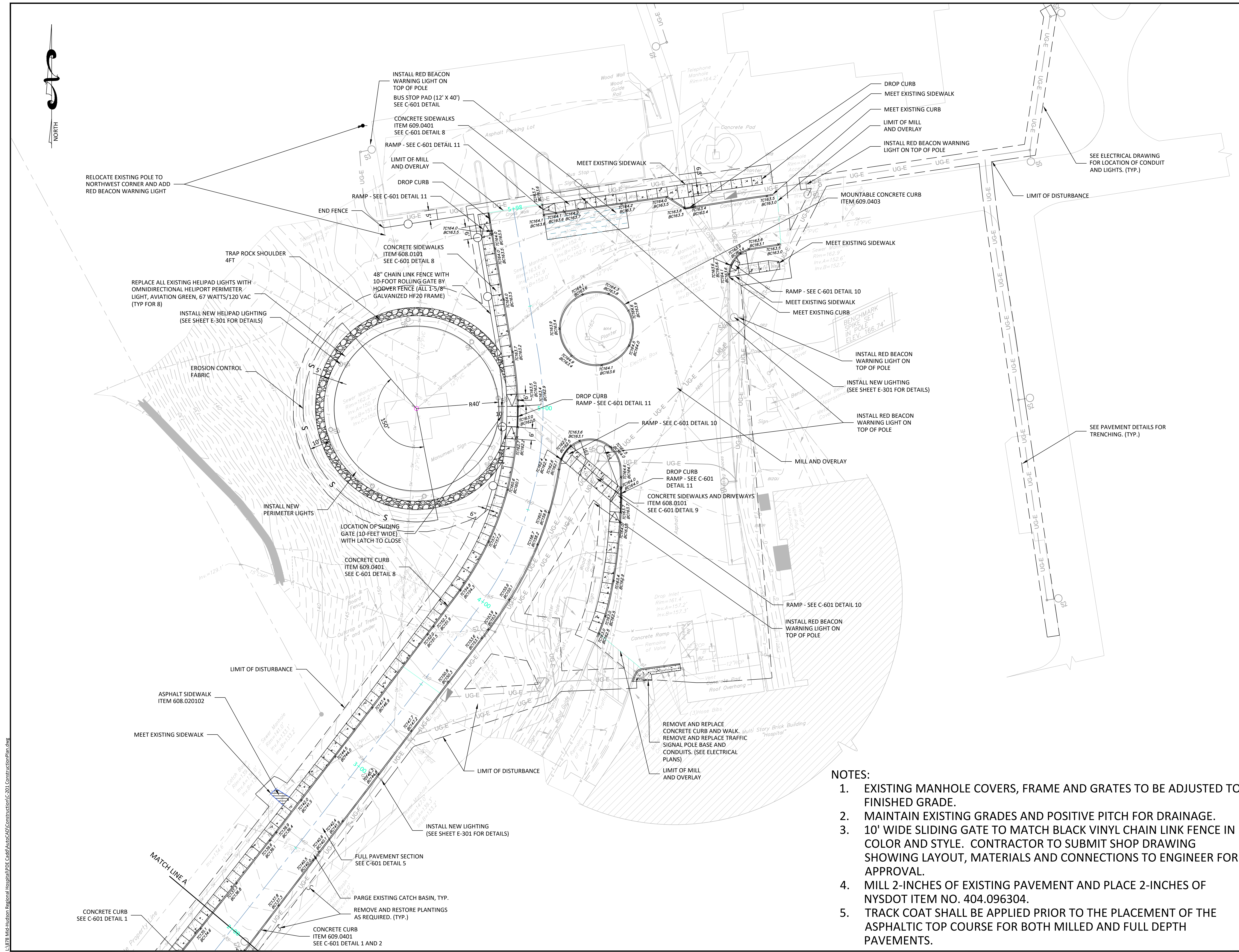
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| Date:        | 07/27/22 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 4 of 22  |
| Dwg. No.:    | C-202    |



EXPIRES 1/31/24

**NOTES:**

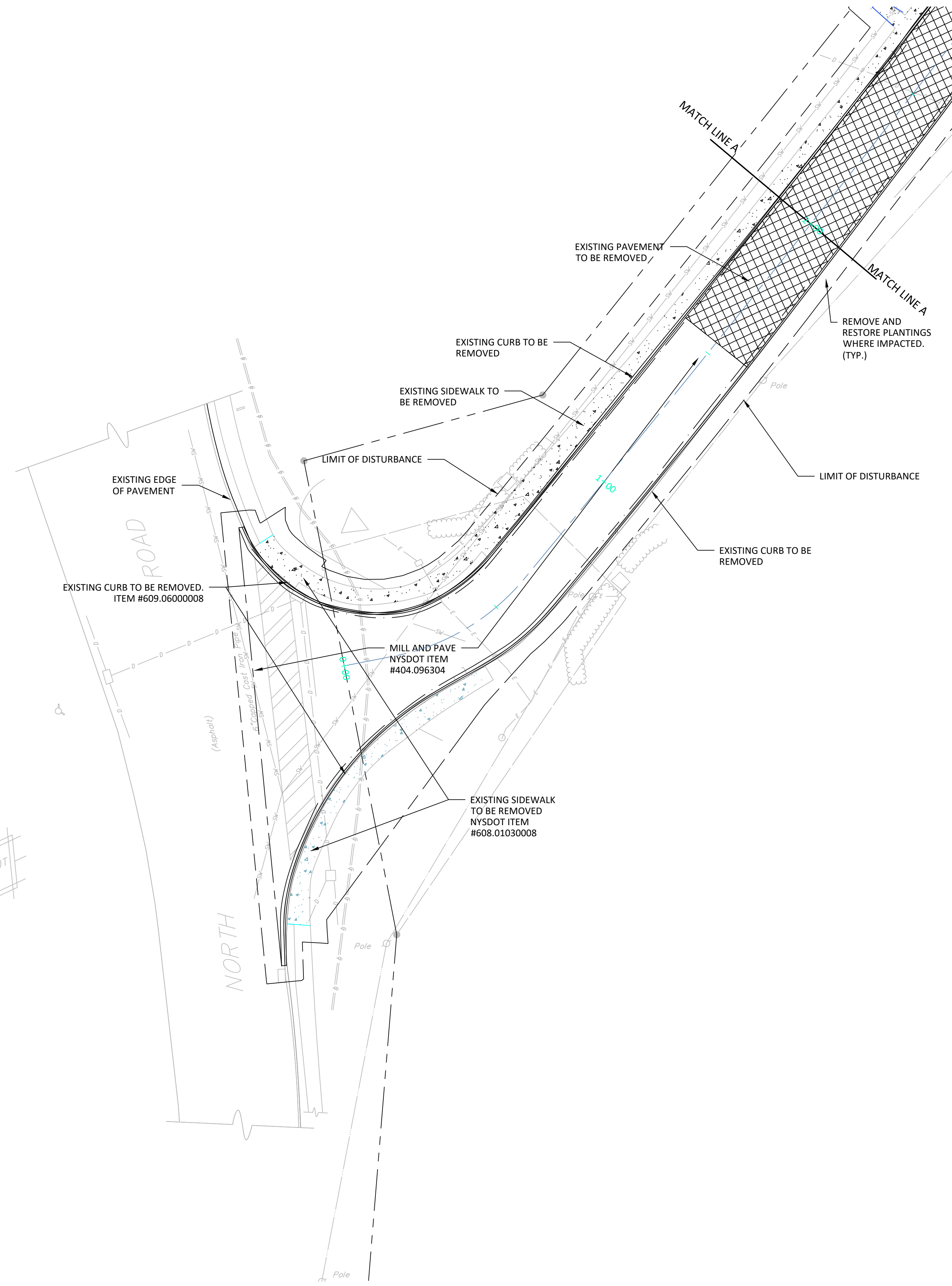
- EXISTING MANHOLE COVERS, FRAME AND GRATES TO BE ADJUSTED TO FINISHED GRADE.
- MAINTAIN EXISTING GRADES AND POSITIVE PITCH FOR DRAINAGE.
- 10' WIDE SLIDING GATE TO MATCH BLACK VINYL CHAIN LINK FENCE IN COLOR AND STYLE. CONTRACTOR TO SUBMIT SHOP DRAWING SHOWING LAYOUT, MATERIALS AND CONNECTIONS TO ENGINEER FOR APPROVAL.
- MILL 2-INCHES OF EXISTING PAVEMENT AND PLACE 2-INCHES OF NYS DOT ITEM NO. 404.096304.
- TRACK COAT SHALL BE APPLIED PRIOR TO THE PLACEMENT OF THE ASPHALTIC TOP COURSE FOR BOTH MILLED AND FULL DEPTH PAVEMENTS.



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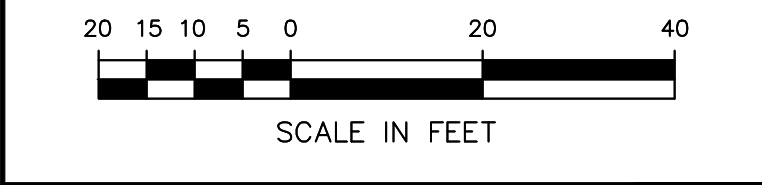
BENCHMARK  
TOP CENTER NUT  
OF HYDRANT  
ELEV. 121.67



| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | REVISIONS PER OGS COMMENTS    | 03/08/2023 |
| 2   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 3   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 4   | PERMIT SET                    | 09/06/2023 |
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**REMOVALS PLAN LEGEND**

PAVEMENT REMOVAL  
 EXISTING CURB  
 REMOVE EXISTING SIGN  
 PAVEMENT SAWCUT LINE  
 SILT FENCE/SEDIMENT BARRIER  
 GRASS AREA REMOVAL  
 CONCRETE WALK REMOVAL  
 REMOVE EXISTING CURB



**MID-HUDSON  
REGIONAL HOSPITAL**  
POUGHKEEPSIE, NEW YORK

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 MEDICAL CENTER

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 241 North Road  
 Poughkeepsie (T), New York  
 WMC CONTRACT NUMBER - WMC-3829  
 OGS PROJECT NUMBER SE391

|               |          |                      |
|---------------|----------|----------------------|
| <b>TITLE:</b> |          | <b>REMOVALS PLAN</b> |
| Scale:        | 1"=20'   |                      |
| Date:         | 07/27/22 |                      |
| Drawn By:     | DC       |                      |
| Checked By:   | GMS      |                      |
| Project No.:  | 0878     |                      |
| Sheet No.:    | 5 of 22  |                      |
| Dwg. No.:     | C-301    |                      |
| Expires:      | 1/31/24  |                      |





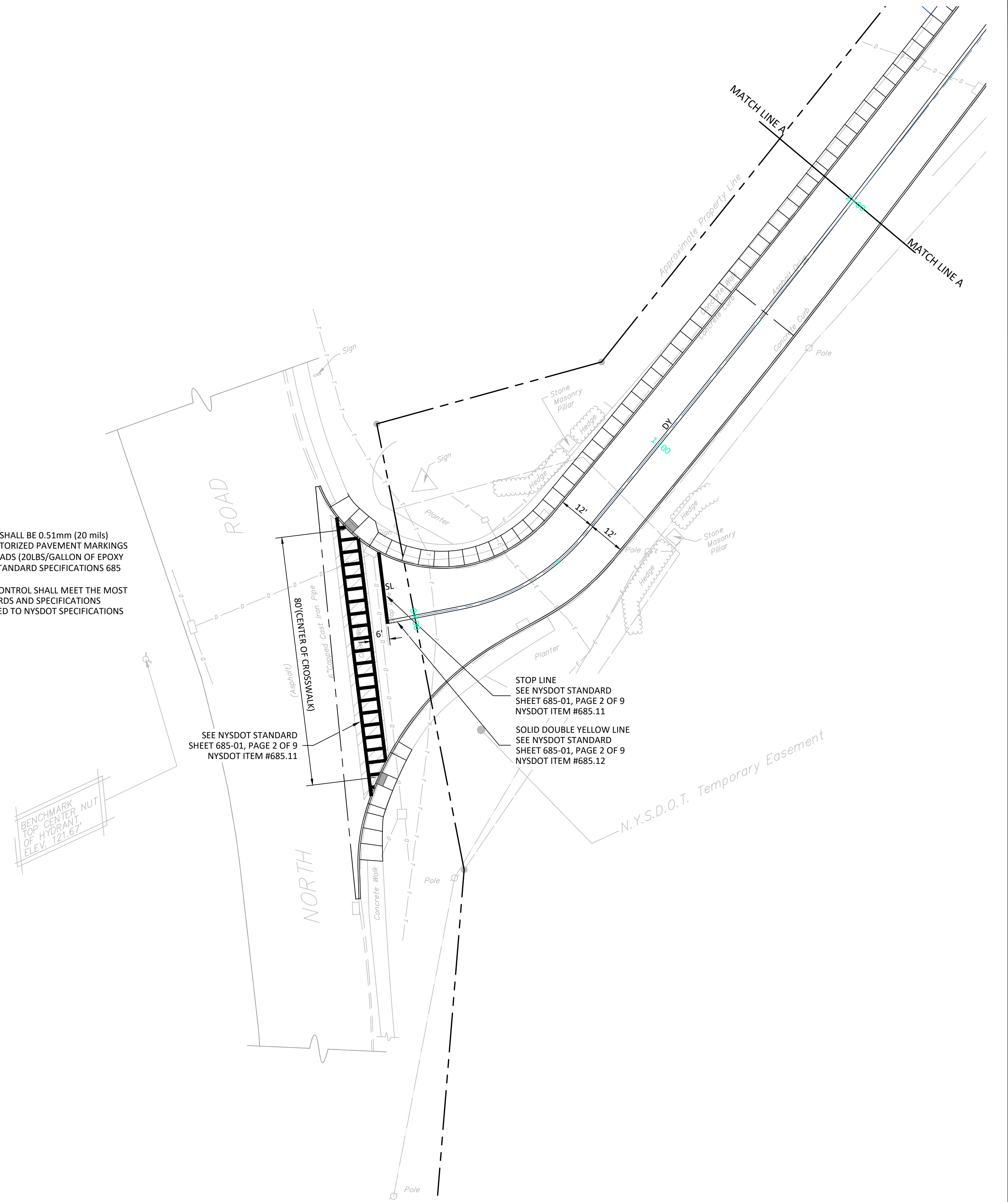
| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | SITE PLAN SUBMISSION          | 04/24/2023 |
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**STRIPING & SIGN PLAN LEGEND**

|     |                                                                            |
|-----|----------------------------------------------------------------------------|
| BW  | BROKEN WHITE LANE LINE, 4"<br>(10' STRIPE/30' SPACE)                       |
| SW  | SOLID WHITE LANE/EDGE LINE, 4"                                             |
| SY  | SOLID YELLOW LANE/EDGE LINE, 4"                                            |
| SL  | SOLID WHITE LINE, 12" WHITE                                                |
| DY  | SOLID DOUBLE YELLOW LINE<br>(2) - 4" YELLOW LINES                          |
| YL  | SOLID YELLOW HATCH LINE<br>(12" WIDE, 3'-0" O.C. @ 45°)                    |
| ⓪   | EXISTING SIGN TO REMAIN                                                    |
| ⓪   | EXISTING SIGN TO BE REMOVED                                                |
| ⓪   | PROPOSED TRAFFIC SIGN<br>LOCATION AND DESIGNATION                          |
| YSL | SOLID YELLOW HATCH LINE<br>(24" WIDE, 8'-0" SPACING<br>AT CURB LINE @ 30°) |



- NOTE:**
- ALL PAVEMENT MARKINGS SHALL BE 0.51mm (20 mils) THICK EPOXY PAINT REFLECTORIZED PAVEMENT MARKINGS WITH REFLECTIVE GLASS BEADS (20LBS/GALLON OF EPOXY RESIN) MEETING NYSDOT STANDARD SPECIFICATIONS 685 AND 727-05
  - ALL WORK ZONE TRAFFIC CONTROL SHALL MEET THE MOST CURRENT NYSDOT STANDARDS AND SPECIFICATIONS INCLUDING BUT NOT LIMITED TO NYSDOT SPECIFICATIONS SECTION 619



SEE NYSDOT STANDARD SHEET 685-01, PAGE 2 OF 9 NYSDOT ITEM #685.11

STOP LINE  
SEE NYSDOT STANDARD SHEET 685-01, PAGE 2 OF 9 NYSDOT ITEM #685.11

SOLID DOUBLE YELLOW LINE  
SEE NYSDOT STANDARD SHEET 685-01, PAGE 2 OF 9 NYSDOT ITEM #685.12

BENCHMARK  
TOP CENTER NUT  
OF HYDRANT  
ELEV. 121.67

- NOTE:**
- TEMPORARY ADA PEDESTRIAN ACCESS SHALL BE PROVIDED THROUGHOUT CONSTRUCTION ALONG THE STATE HIGHWAY OR APPROPRIATE PEDESTRIAN DETOURS PROVIDED PER NYSDOT STANDARDS. SEE NYSDOT STANDARD PEDESTRIAN DETOUR PROVIDED IN THE PLAN SET

**MID-HUDSON REGIONAL HOSPITAL**  
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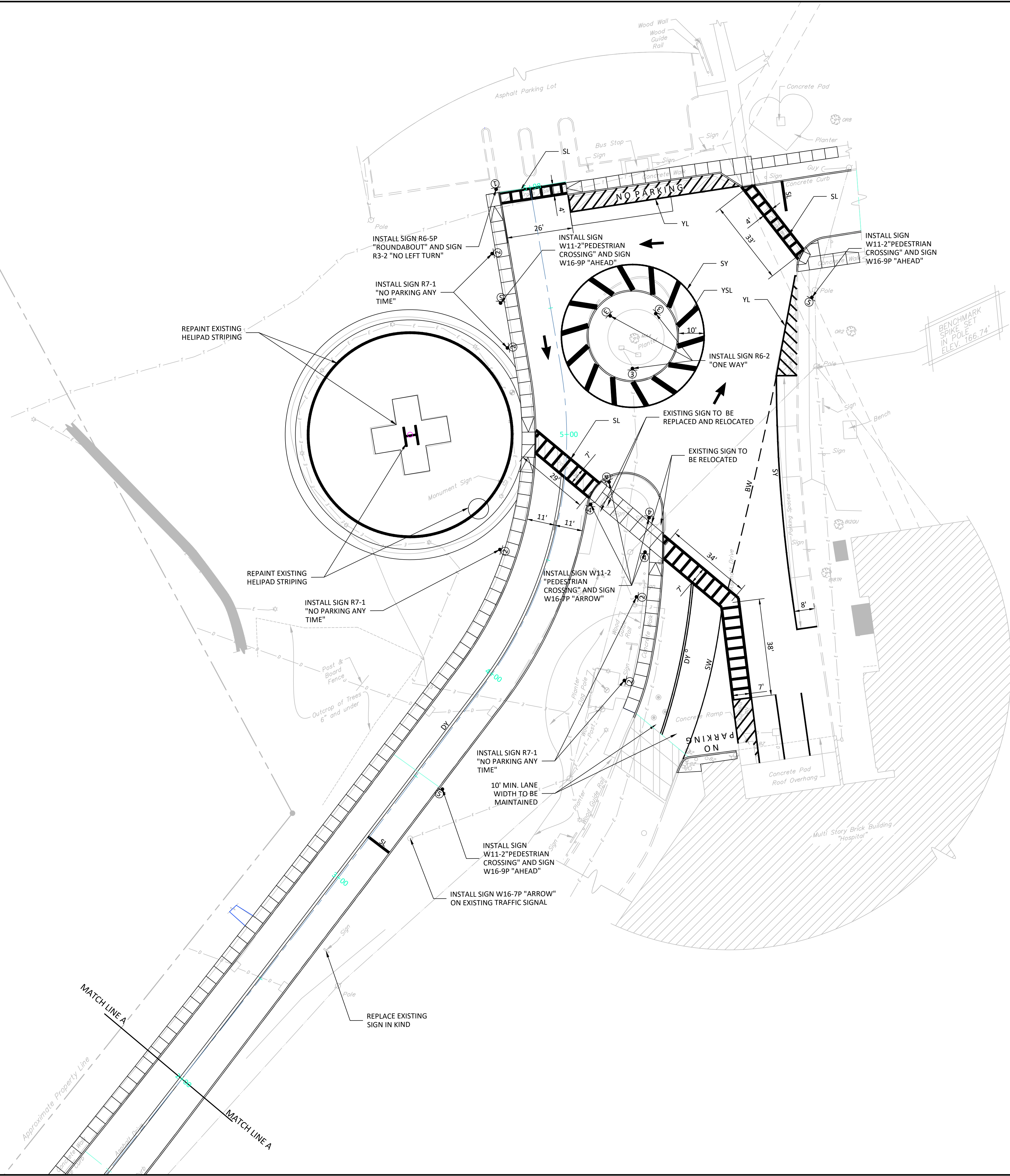
Mid-Hudson Regional Hospital  
241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

TITLE:  
**MARKING AND SIGNAGE PLAN**

|              |          |
|--------------|----------|
| Scale:       | 1"=20'   |
| Date:        | 07/27/22 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 7 of 22  |
| Dwg. No.:    | C-401    |

EXPIRES 1/31/24

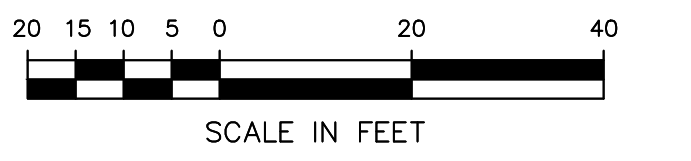




| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS   | 06/22/2023 |
| 3   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 4   | PERMIT SET                    | 09/06/2023 |

**STRIPING & SIGN PLAN LEGEND**

|     |                                                                            |
|-----|----------------------------------------------------------------------------|
| BW  | BROKEN WHITE LANE LINE, 4"<br>(10' STRIPE/30' SPACE)                       |
| SW  | SOLID WHITE LANE/EDGE LINE, 4"                                             |
| SY  | SOLID YELLOW LANE/EDGE LINE, 4"                                            |
| SL  | SOLID WHITE LINE, 12" WHITE                                                |
| DY  | SOLID DOUBLE YELLOW LINE<br>(2) - 4" YELLOW LINES                          |
| YL  | SOLID YELLOW HATCH LINE<br>(12" WIDE, 3'-0" O.C. @ 45°)                    |
| Ⓢ   | EXISTING SIGN TO REMAIN                                                    |
| Ⓧ   | EXISTING SIGN TO BE REMOVED                                                |
| Ⓛ   | PROPOSED TRAFFIC SIGN<br>LOCATION AND DESIGNATION                          |
| YSL | SOLID YELLOW HATCH LINE<br>(24" WIDE, 8'-0" SPACING<br>AT CURB LINE @ 30°) |



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Valhalla, NY 10595

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241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

TITLE:  
**MARKING AND SIGNAGE PLAN**

|              |          |
|--------------|----------|
| Scale:       | 1"=20'   |
| Date:        | 07/27/22 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 22 of 14 |
| Dwg. No.:    | C-402    |

EXPIRES 1/31/24

L:\878 Mid-Hudson Regional Hospital\PIE Credit\AutoCAD\Construction\C-401 MarkingSignPlan.dwg



| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | REVISIONS PER OGS COMMENTS    | 03/08/2023 |
| 2   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 3   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 4   | PERMIT SET                    | 09/06/2023 |
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EROSION CONTROL PLAN LEGEND

- INLET PROTECTION/FILTER
- LIMITS OF DISTURBANCE
- SILTY FENCE/SEDIMENT BARRIER
- SEDIMENT FILTER LOG



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WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

TITLE: **EROSION CONTROL PLAN**

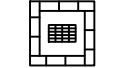
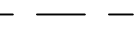
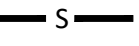

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| Scale:       | 1"=20'   |
| Date:        | 07/27/22 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 9 of 22  |
| Dwg. No.:    | C-501    |

EXPIRES 1/31/24



| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | REVISIONS PER OGS COMMENTS    | 03/08/2023 |
| 2   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 3   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 4   | PERMIT SET                    | 09/06/2023 |
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**EROSION CONTROL PLAN LEGEND**

-  INLET PROTECTION/FILTER
-  LIMITS OF DISTURBANCE
-  SILT FENCE/SEDIMENT BARRIER
-  SEDIMENT FILTER LOG



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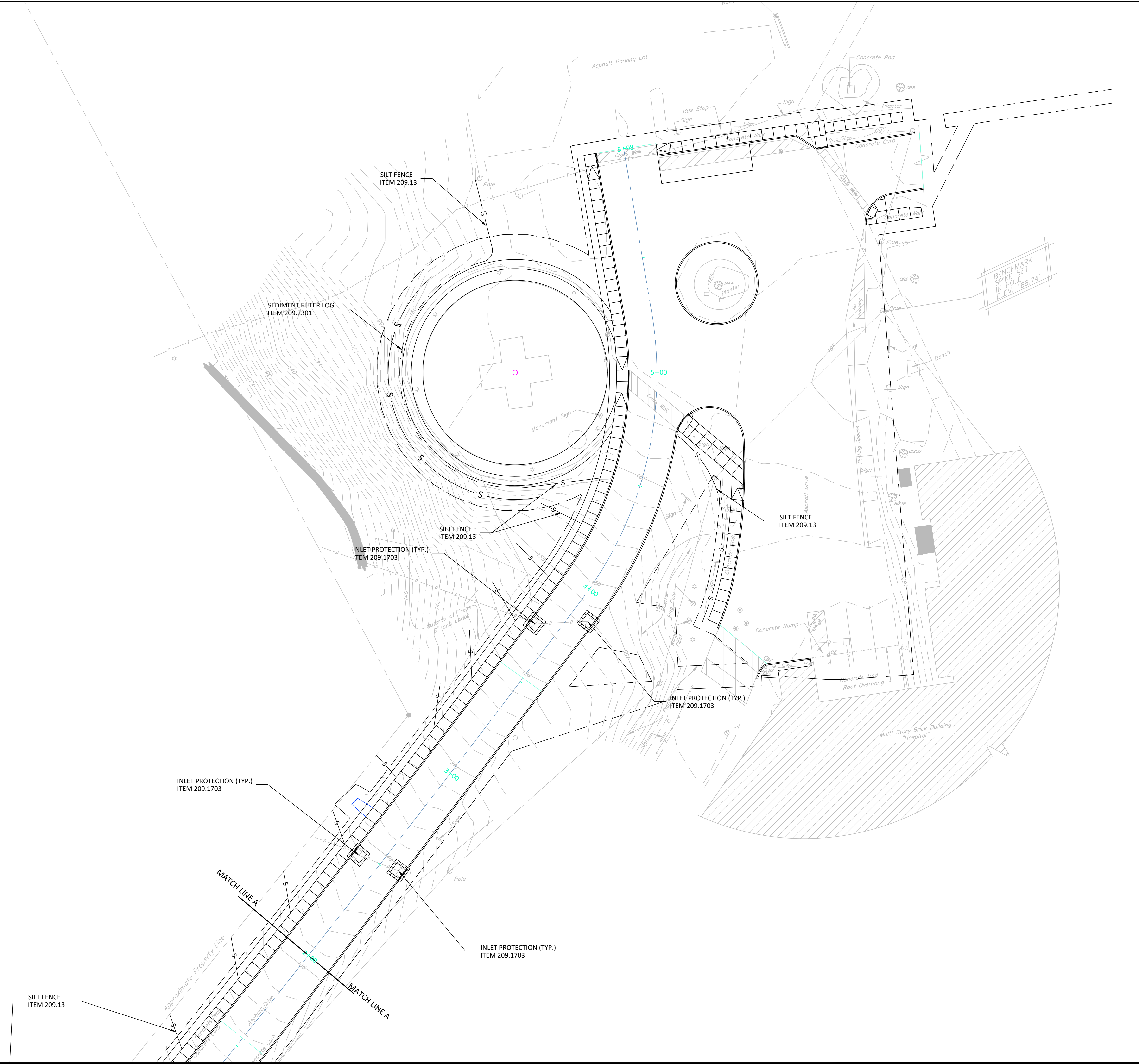
Mid-Hudson Regional Hospital  
241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

TITLE:  
**EROSION CONTROL PLAN**

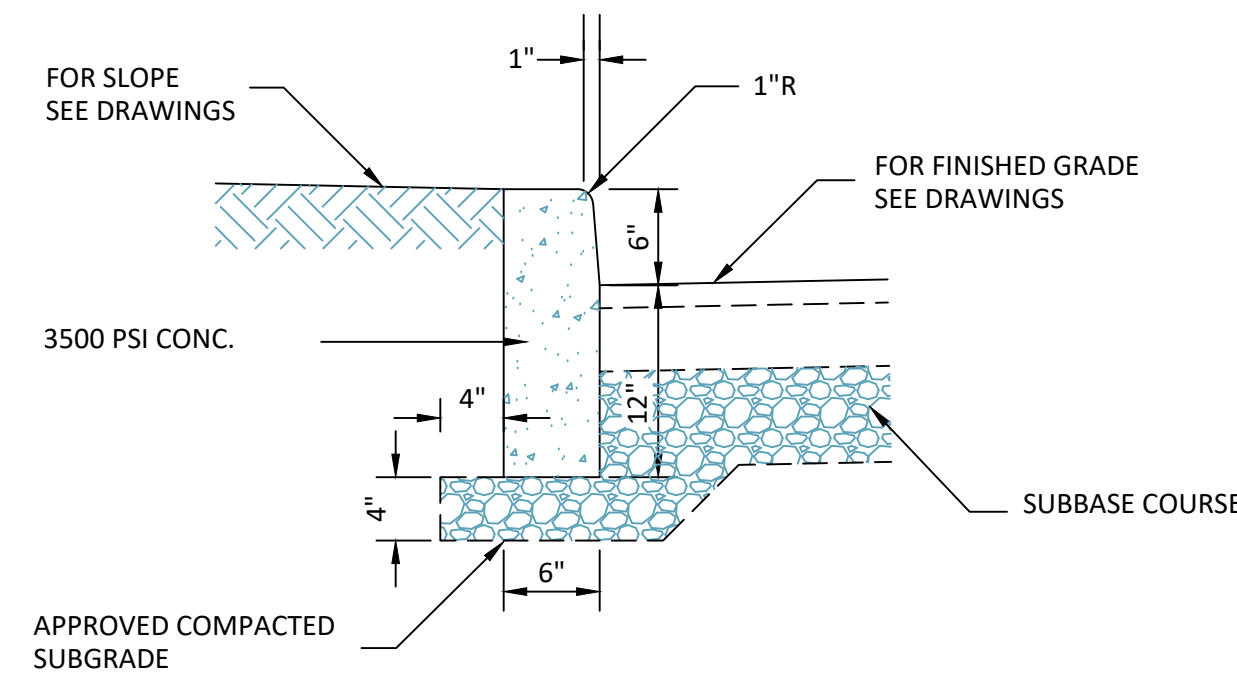
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| Date:        | 07/27/22 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 10 of 22 |
| Dwg. No.:    | C-502    |

EXPIRES 1/31/24

L:\28 Mid-Hudson Regional Hospital\PE\_Cad\AutoCAD\Construction\C-501 ErosionControlPlan.dwg

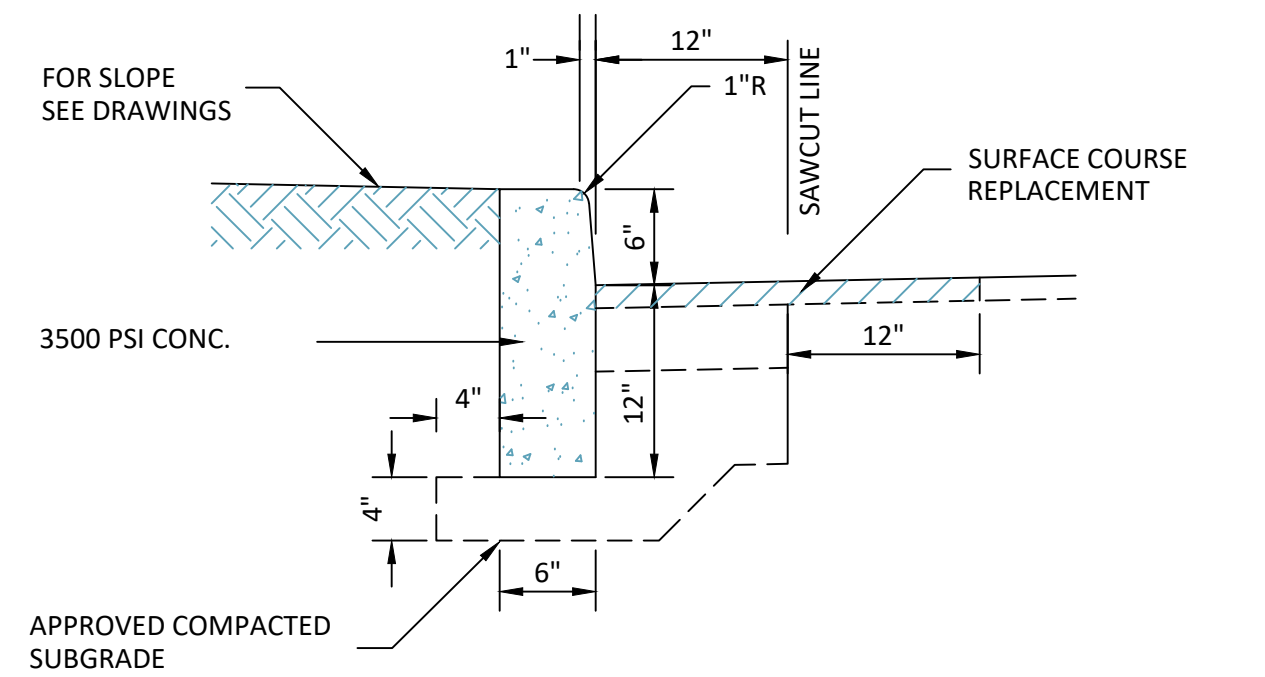


| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 2   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 3   | PERMIT SET                    | 09/06/2023 |



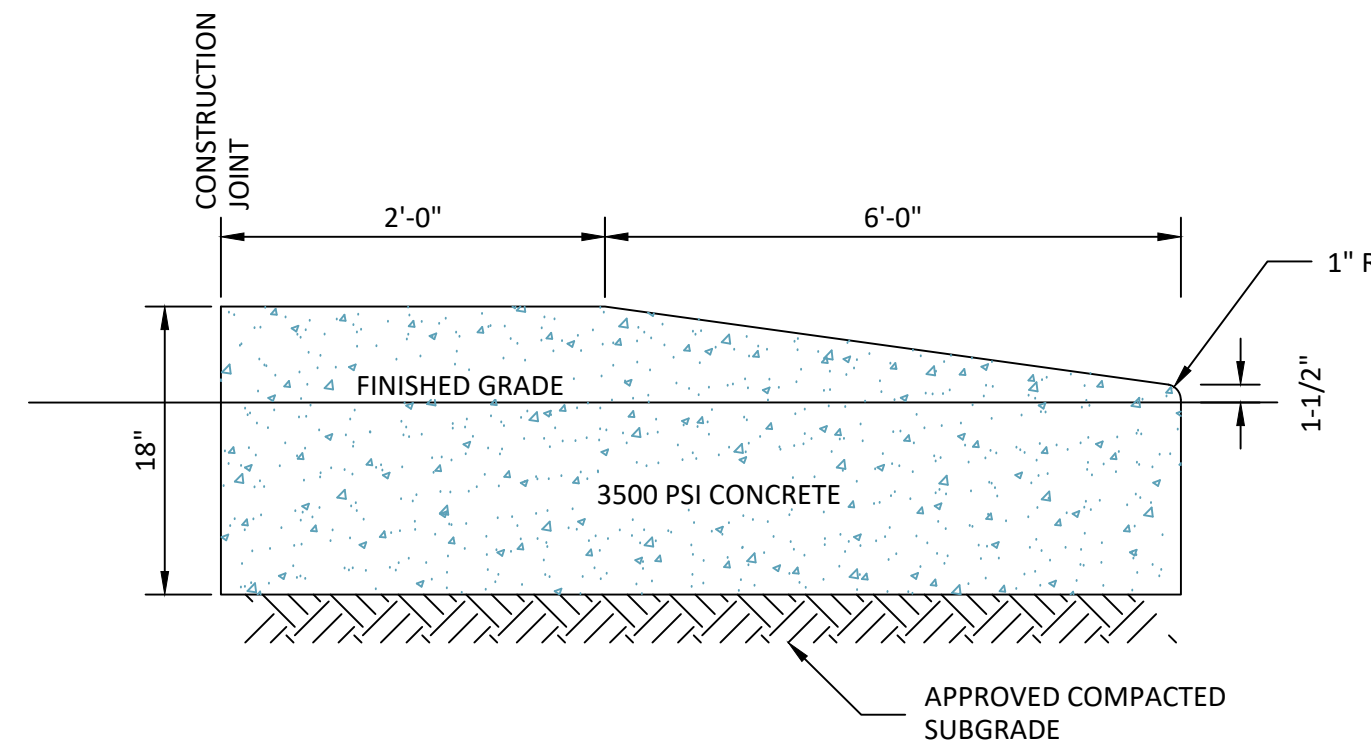
CONCRETE CURB  
NOT TO SCALE

1



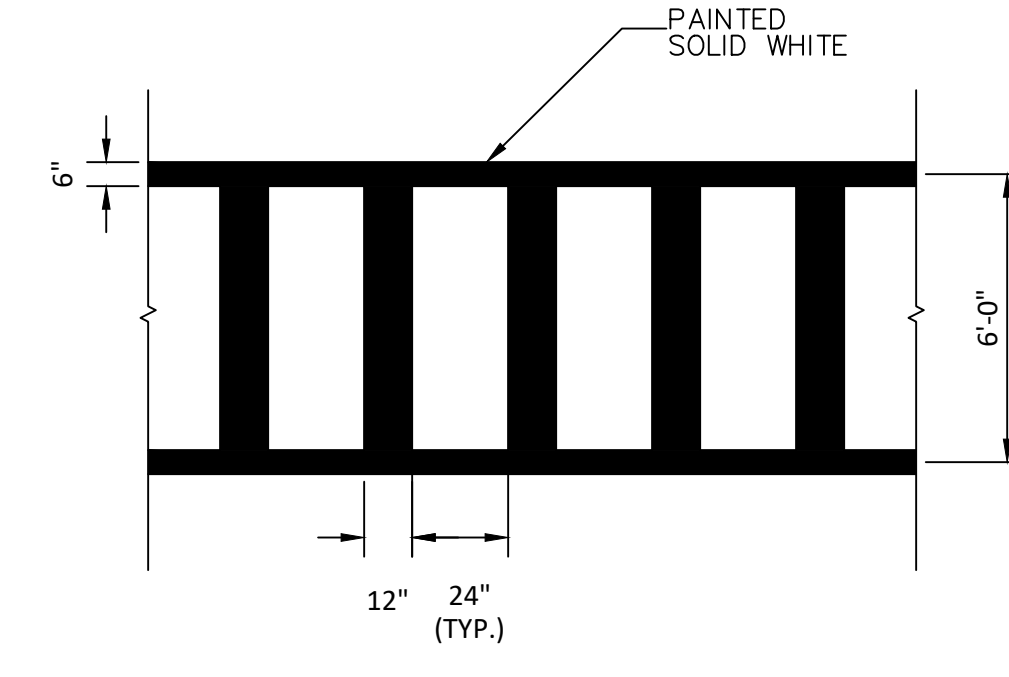
CONCRETE CURB IN EXISTING PAVEMENT  
NOT TO SCALE

2



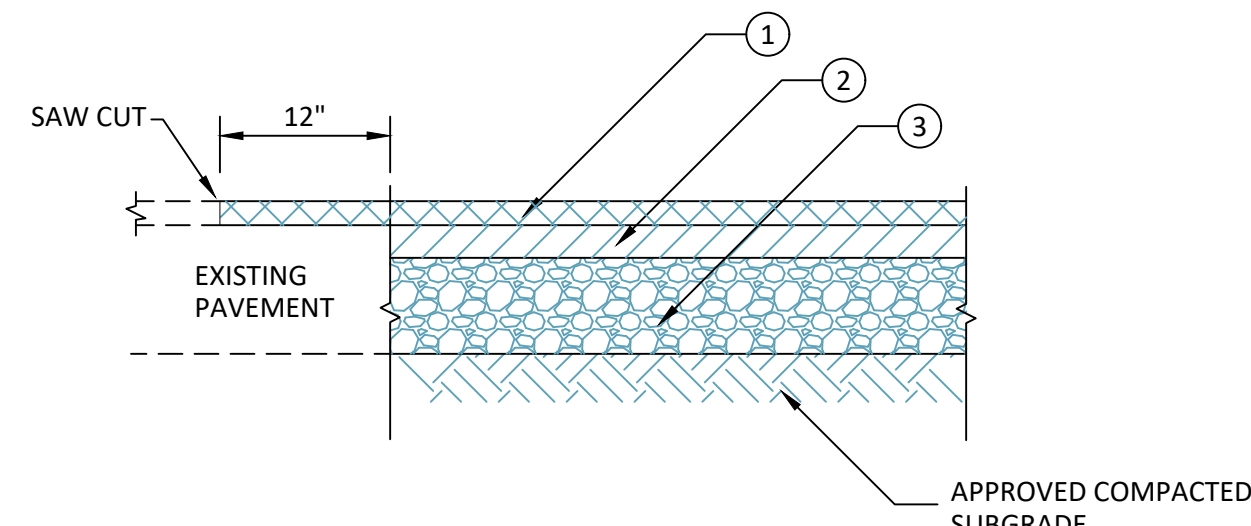
CONCRETE CURB ENDING  
NOT TO SCALE

3



PEDESTRIAN CROSSWALK  
NOT TO SCALE

4

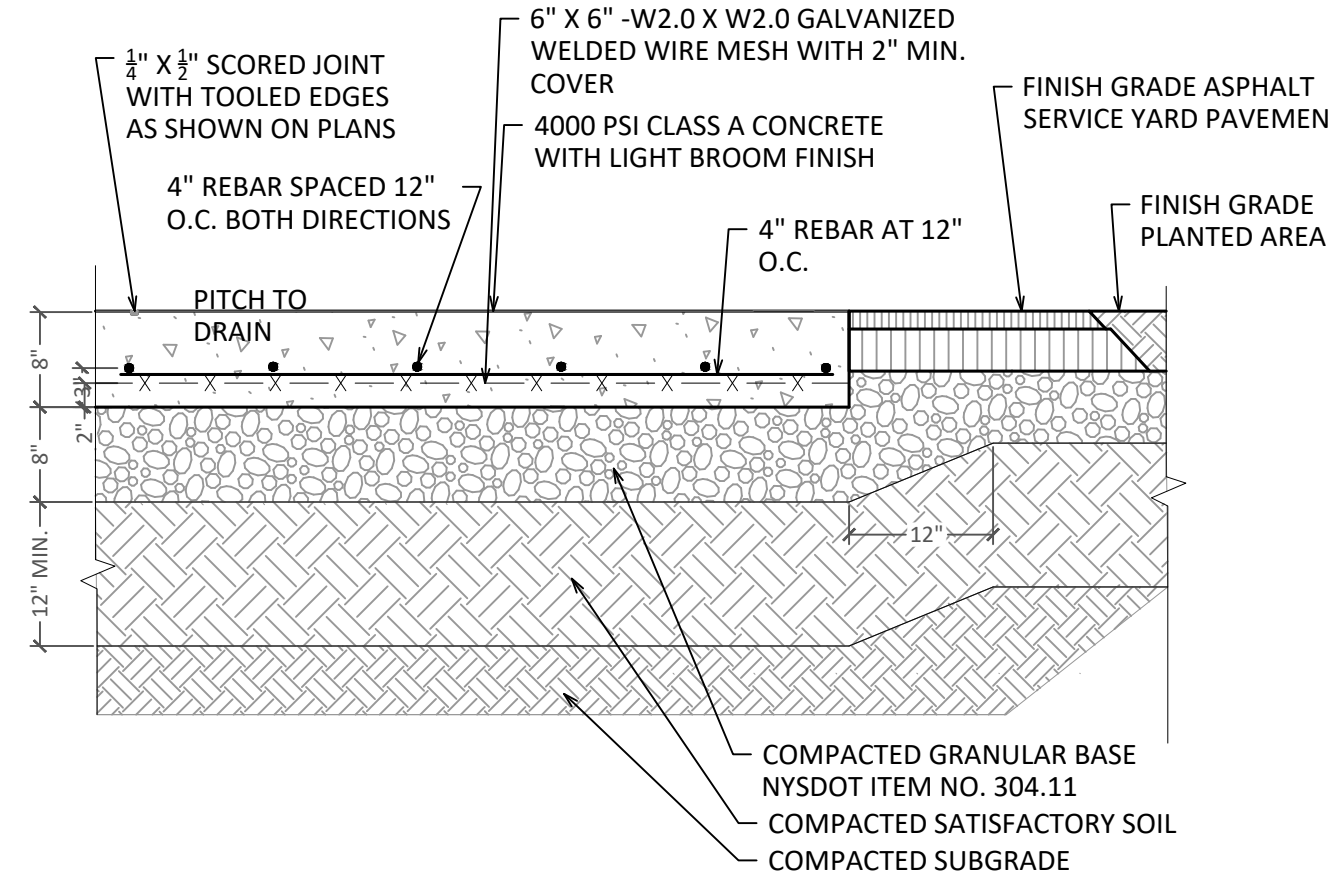


PAVEMENT DESIGN LEGEND:

- 2" TOP COURSE, NYS DOT ITEM 403.1701, TYPE 6F
- 4" BINDER COURSE, NYS DOT ITEM 403.13, TYPE 3
- 6" SUBBASE COURSE, NYS DOT ITEM 304.03, TYPE 2

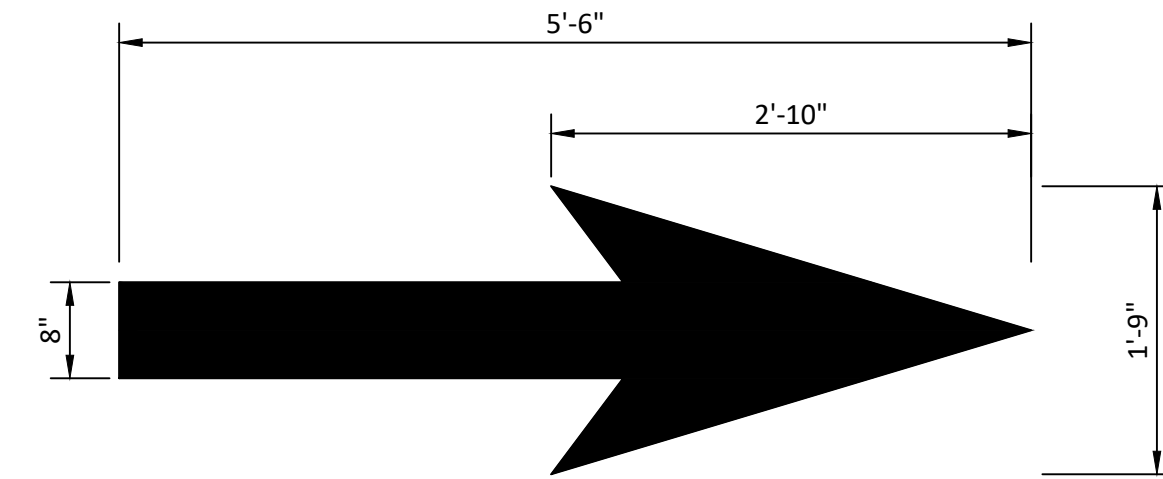
PAVEMENT DESIGN  
NOT TO SCALE

5



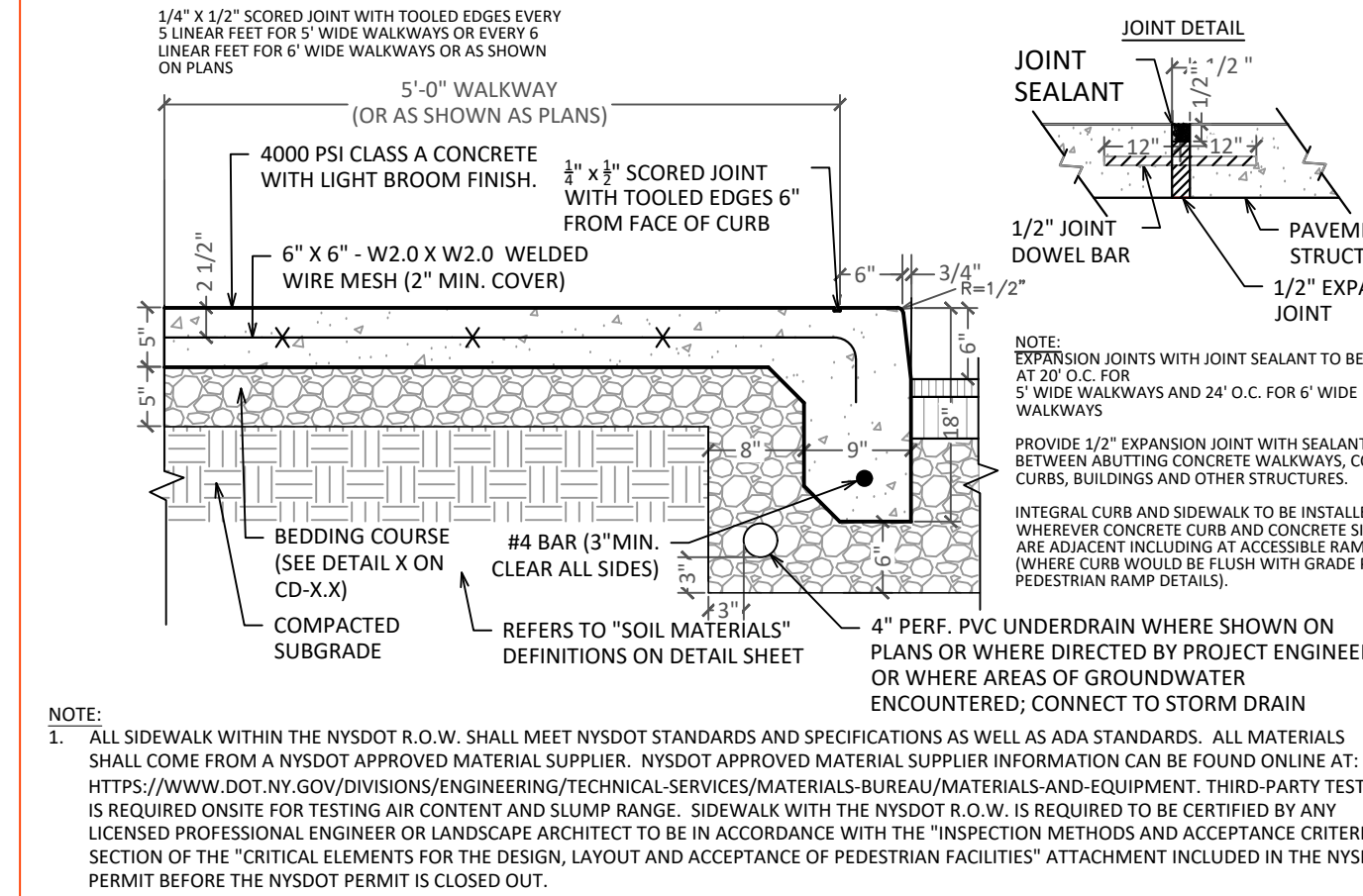
PAVEMENT DESIGN - BUS STOP CONCRETE PAD  
NOT TO SCALE

6



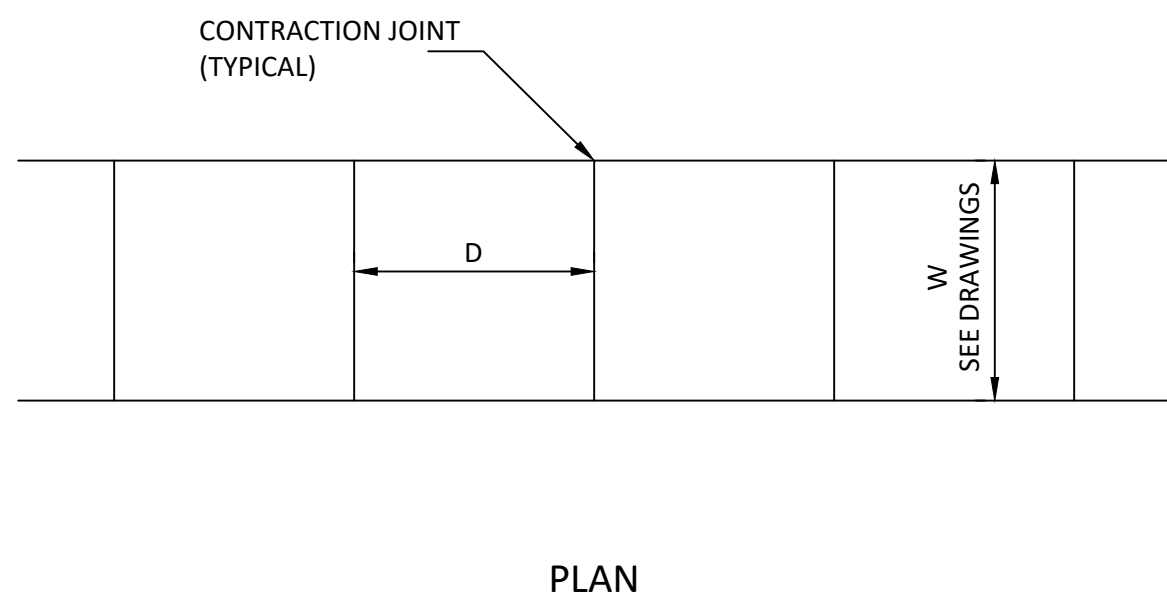
LANE USE ARROWS  
NOT TO SCALE

7

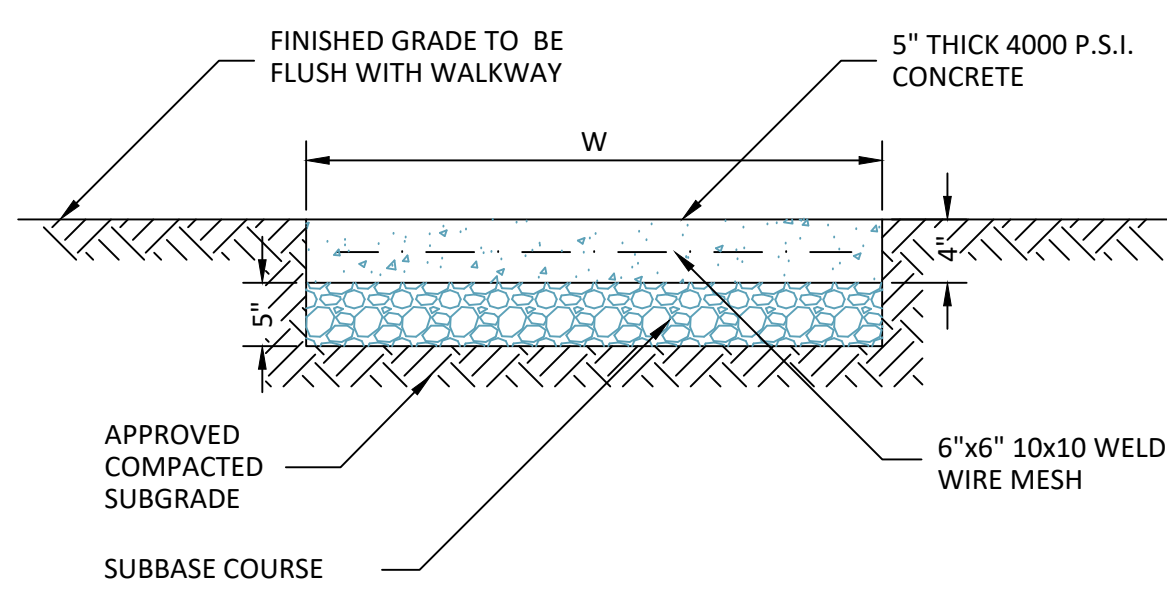


CONCRETE WALK WITH INTEGRAL CURB - TYPE 2  
NOT TO SCALE

8



PLAN



SECTION

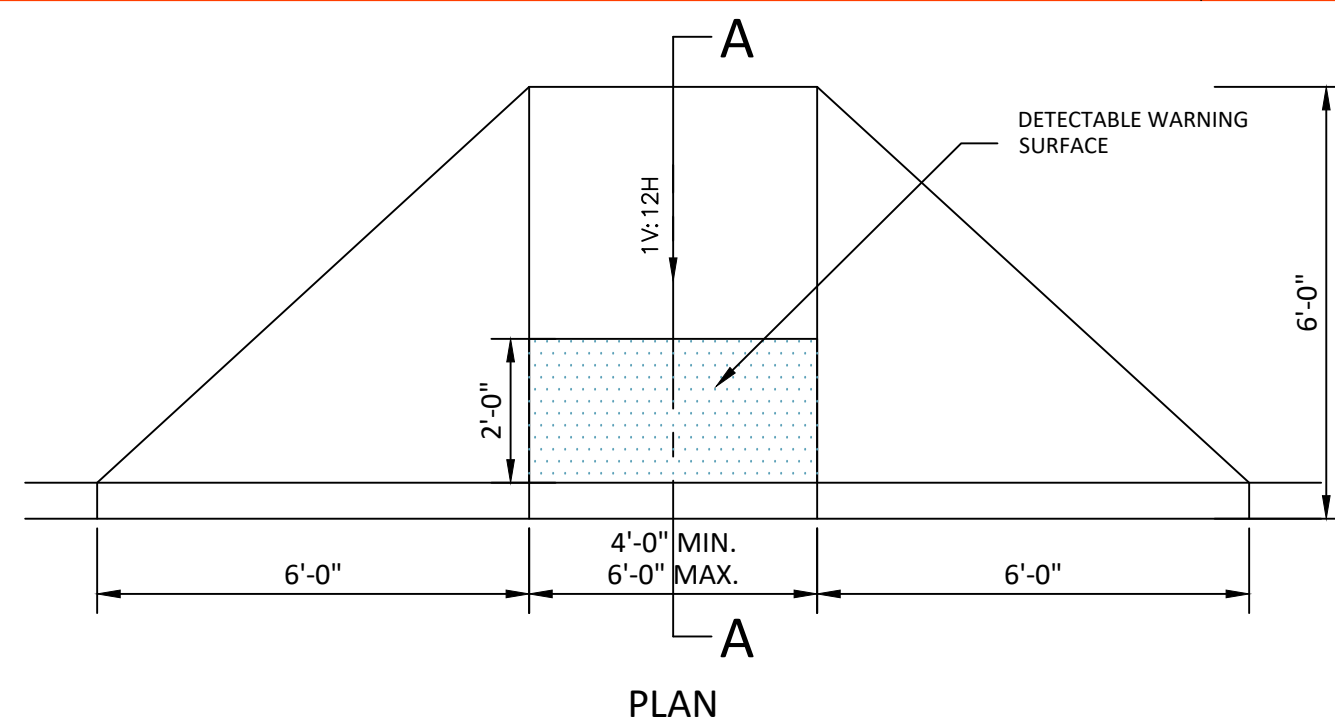
NOTE:

- MINIMUM CROSS SLOPE 1/8"/FT. UNLESS OTHERWISE INDICATED ON THE DRAWINGS.
- PROVIDE 1/4" PRE-MOULDED EXPANSION JOINTS AT 20' INTERVALS.
- MAXIMUM CROSS SLOPE SHALL BE 1.5%

| W   | D  |
|-----|----|
| 3'  | 4' |
| 4'  | 4' |
| 5'  | 5' |
| >5' | 5' |

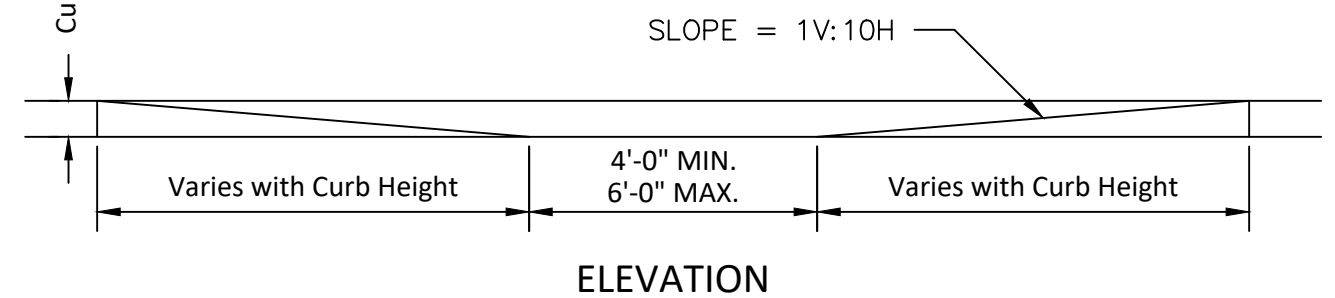
CONCRETE SIDEWALK  
NOT TO SCALE

9

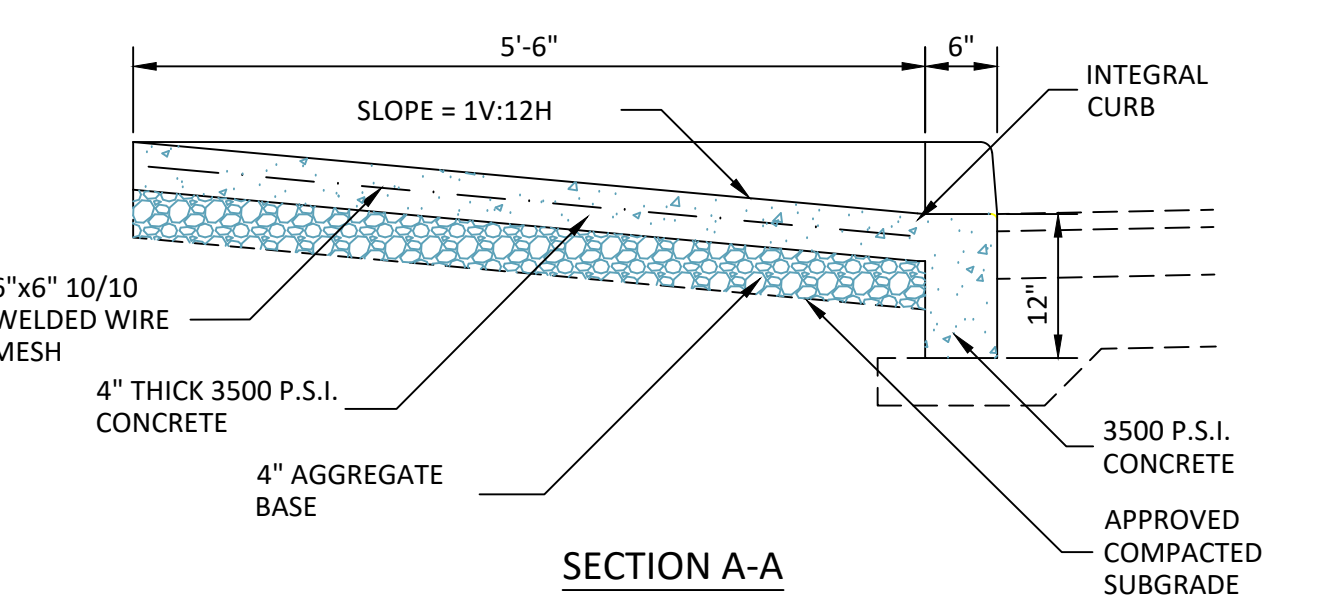


PLAN

NOTE: THE MAXIMUM SLOPE OF THE RAMP SHALL BE 1V:12H



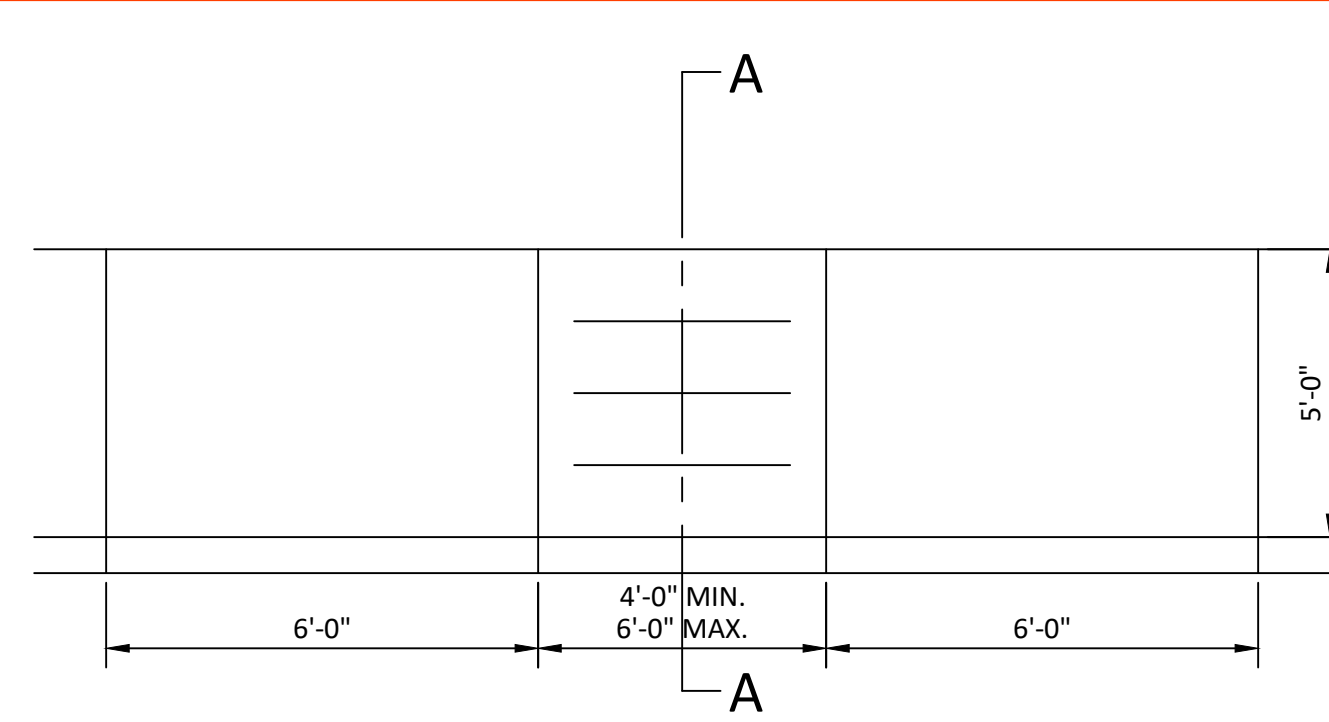
ELEVATION



SECTION A-A

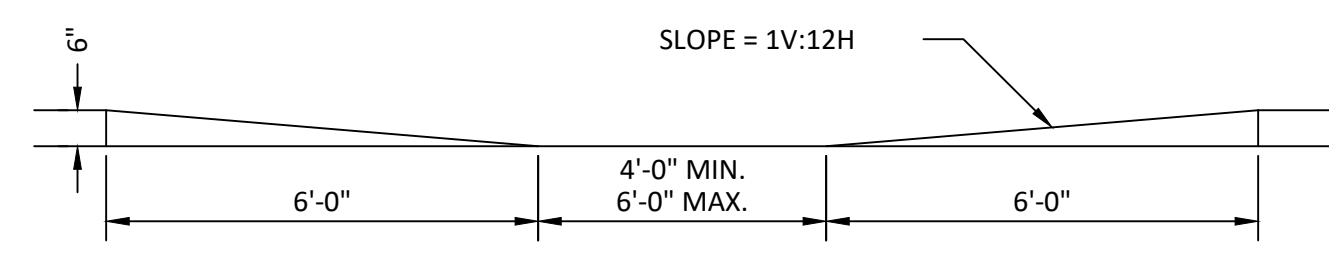
CURB RAMP  
NOT TO SCALE

10

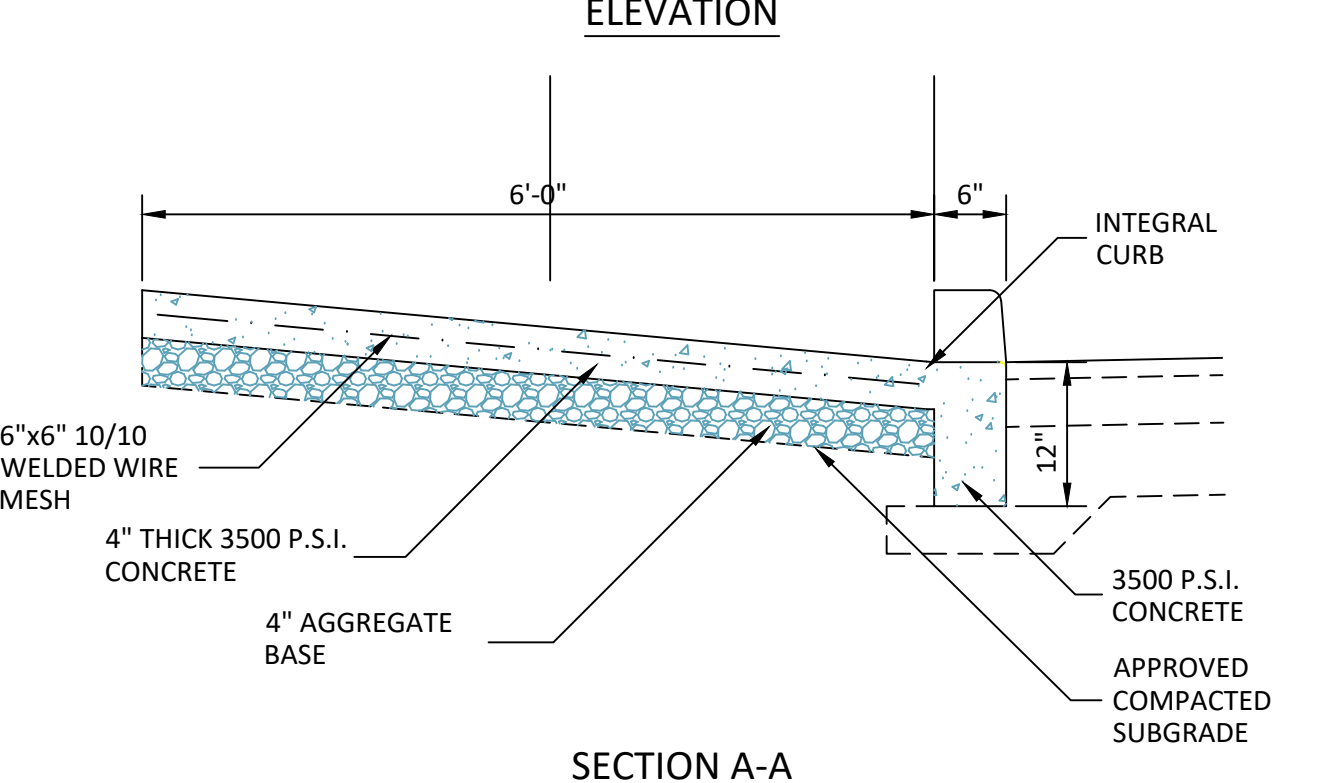


PLAN

NOTE: THE MAXIMUM SLOPE OF THE RAMP SHALL BE 1V:12H



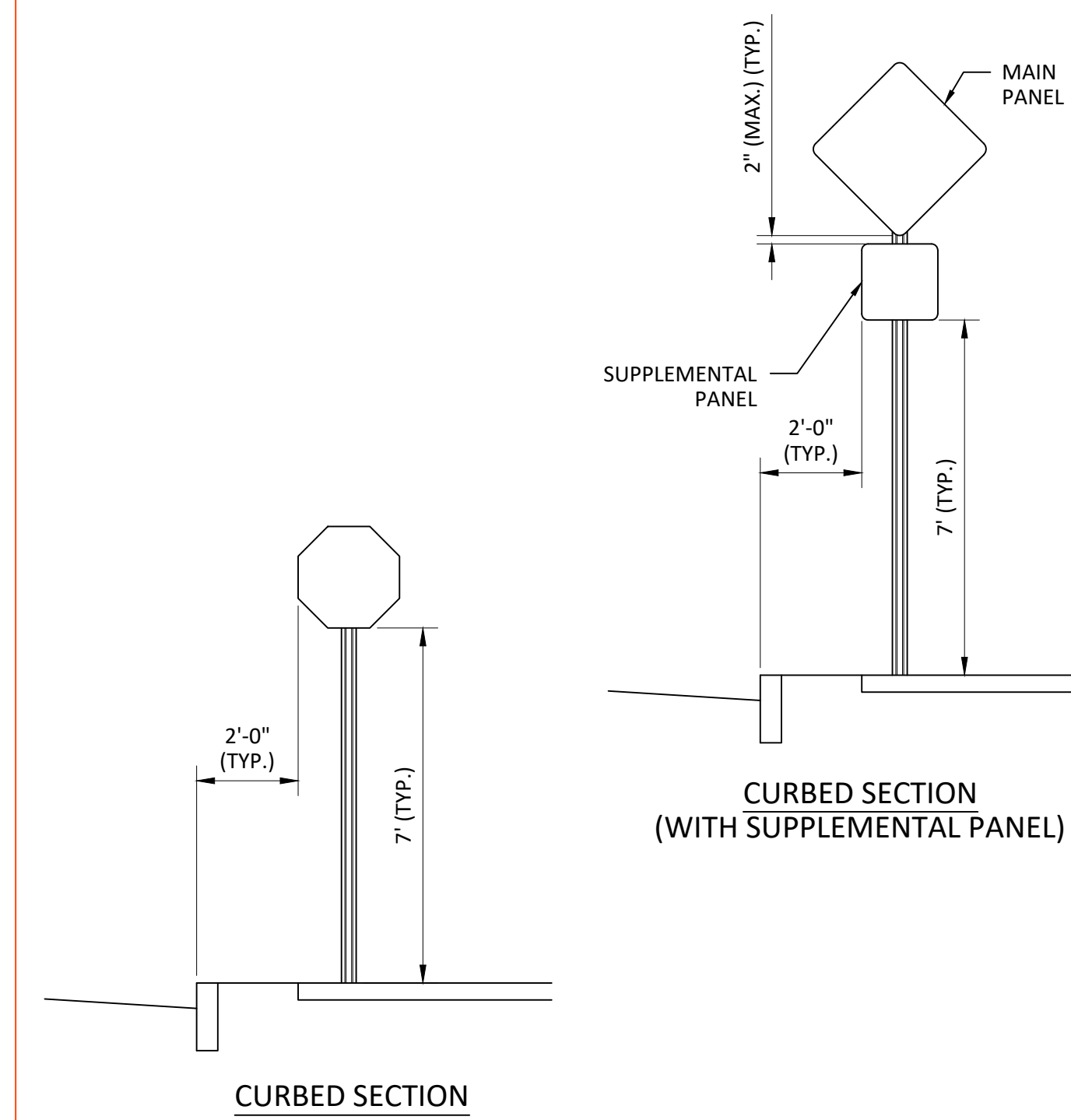
ELEVATION



SECTION A-A

DROP CURB AND SIDEWALK RAMP  
NOT TO SCALE

11



SIGN PLACEMENT DETAIL  
NOT TO SCALE

12

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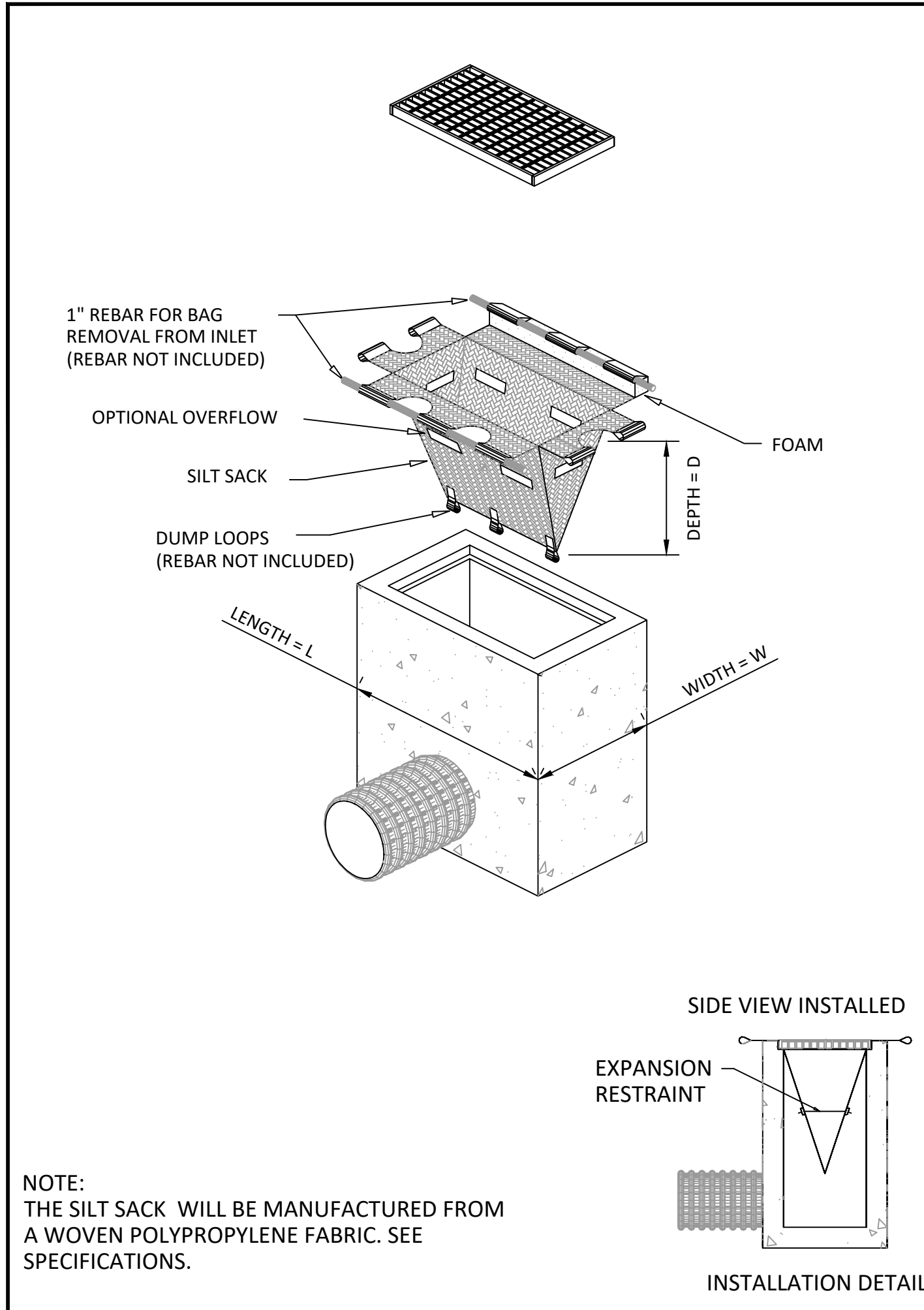
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241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

TITLE: DETAILS

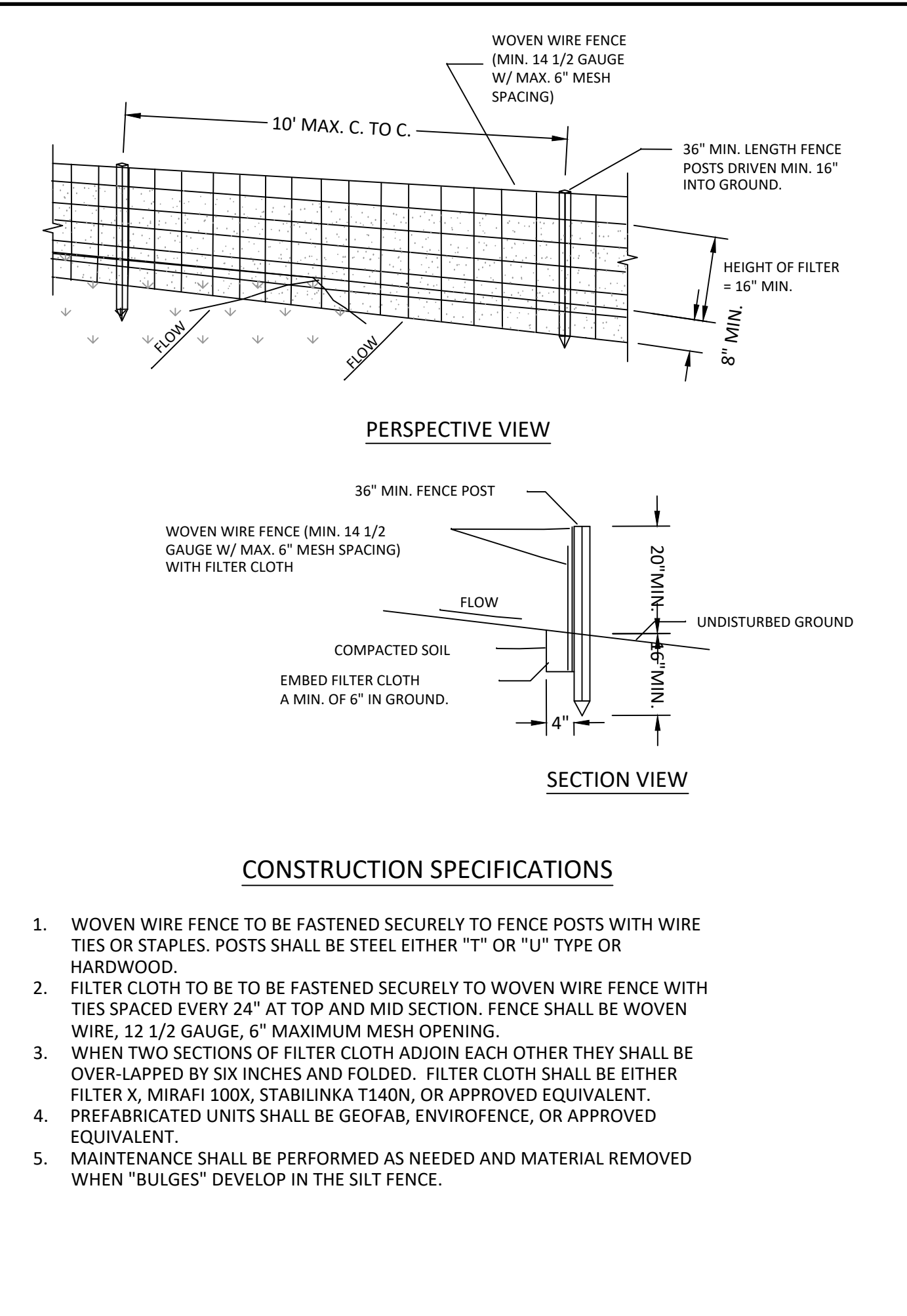
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|--------------|----------|
| Date:        | 7/27/22  |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 11 of 22 |
| Dwg. No.:    | C-601    |

EXPIRES 1/31/24



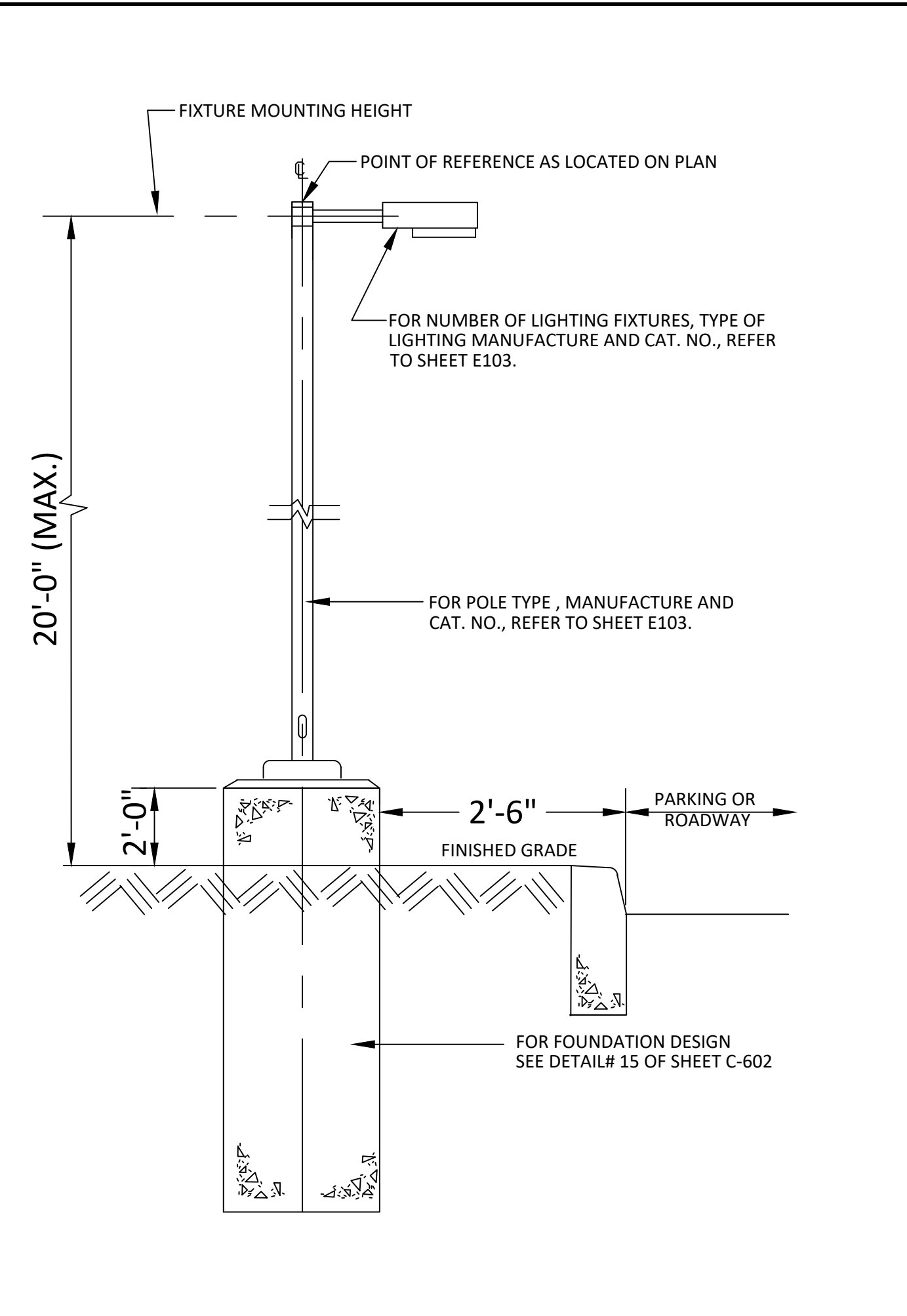
CATCH BASIN SILT SACK (INLET PROTECTION)  
NOT TO SCALE

12



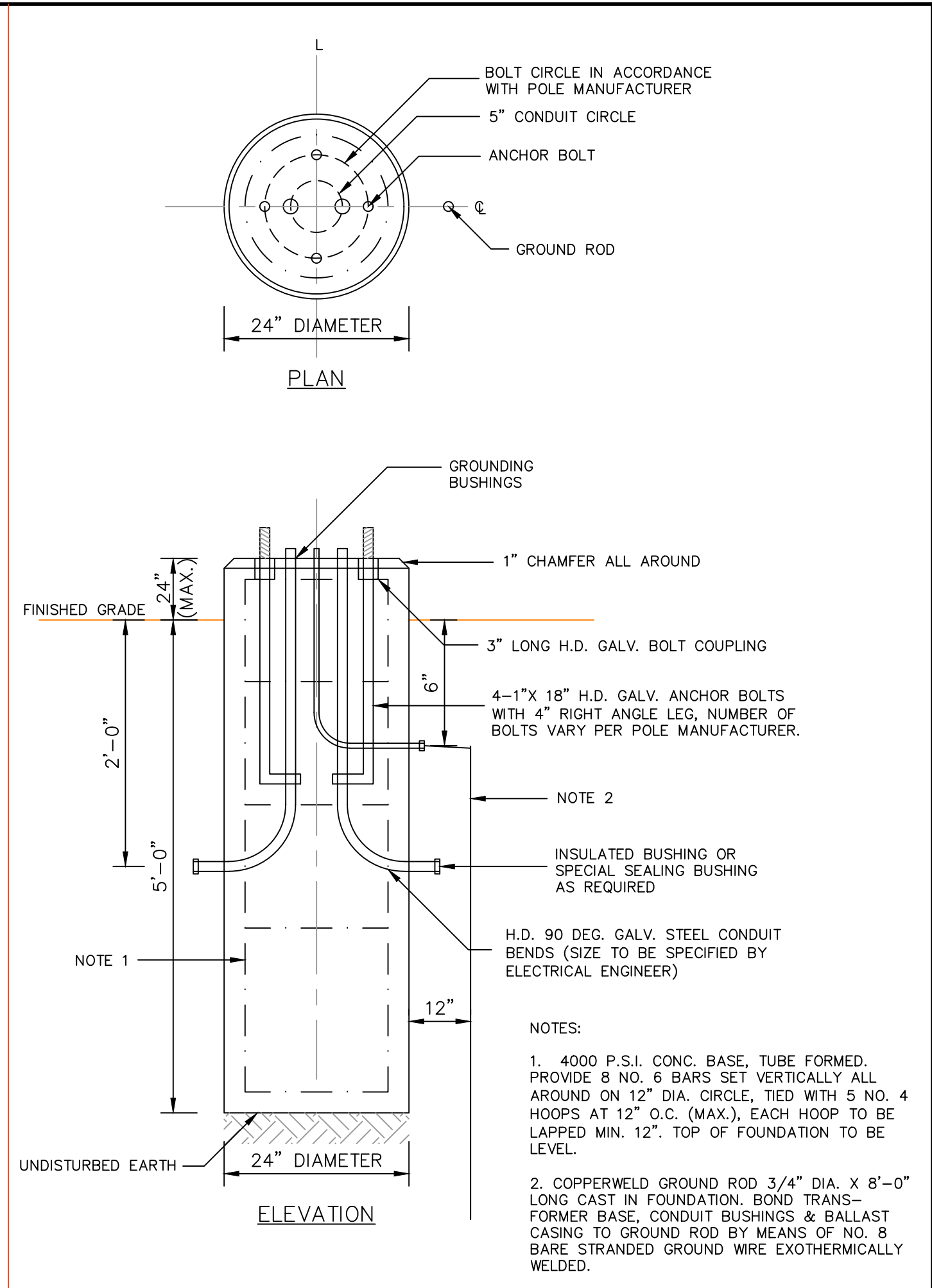
SILT FENCE  
NOT TO SCALE

13



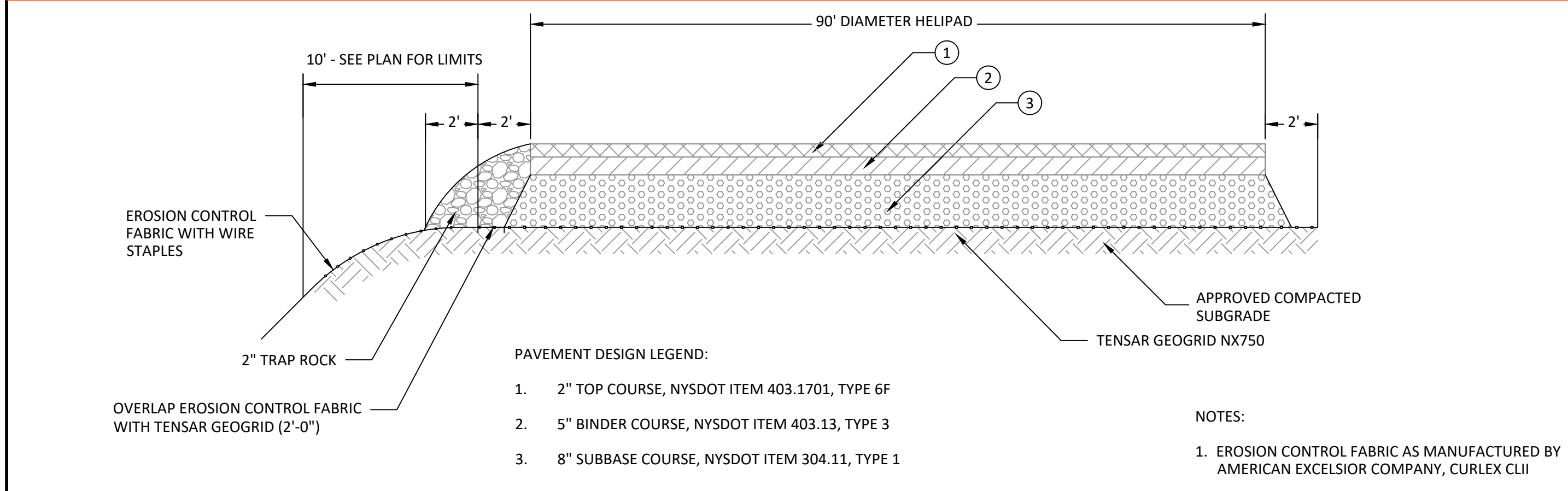
LIGHTING STANDARD & FOUNDATION FOR 20 FOOT MOUNTING HEIGHT  
NOT TO SCALE

14



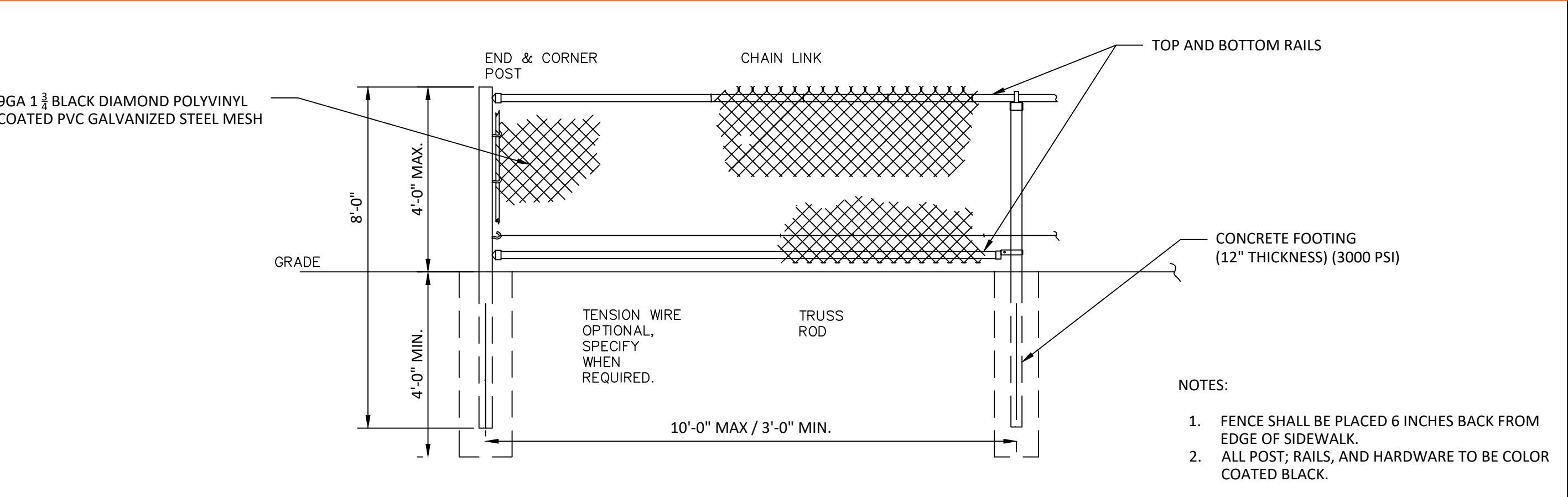
LIGHT POLE FOUNDATION  
NOT TO SCALE

15



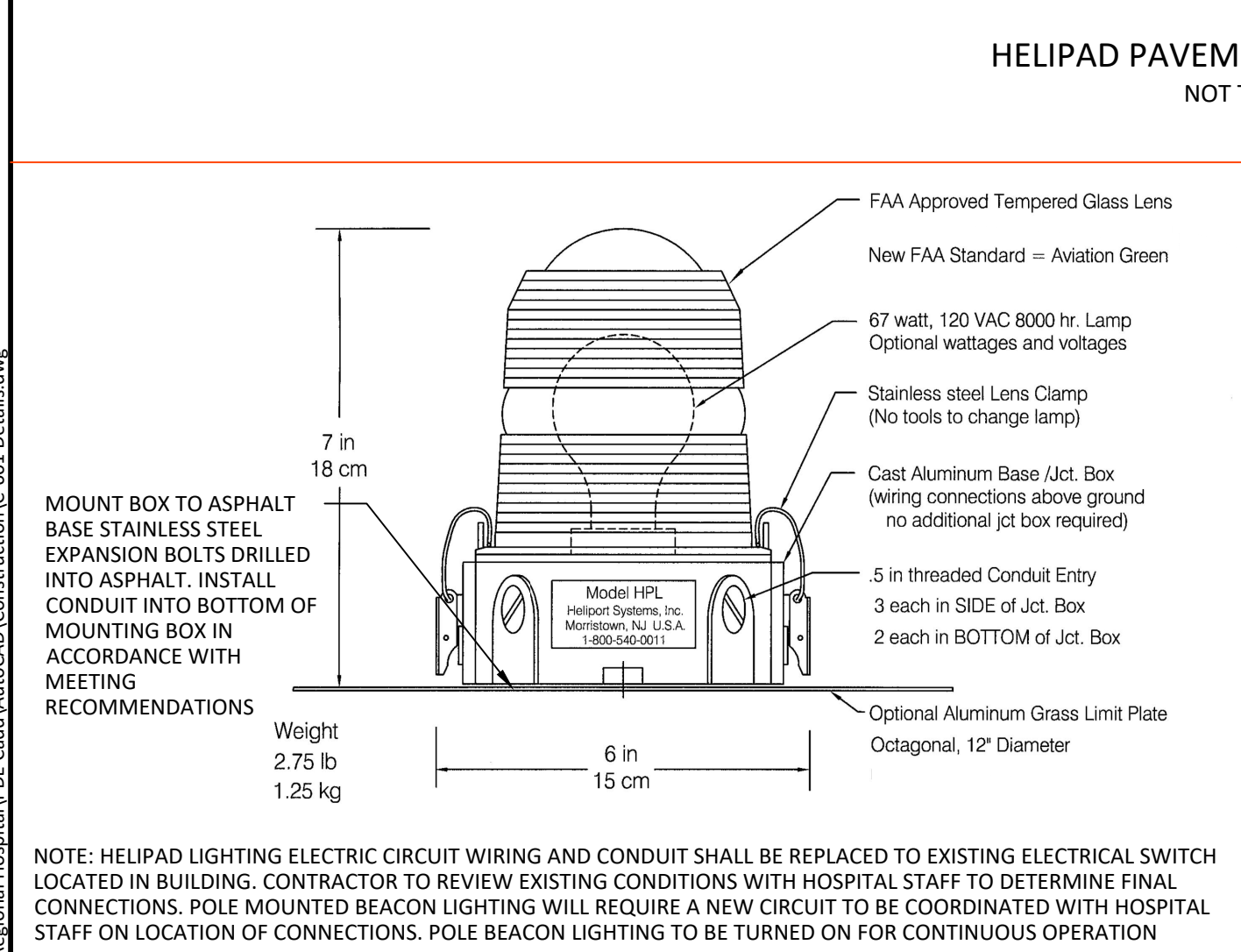
HELIPAD PAVEMENT REPLACEMENT  
NOT TO SCALE

16



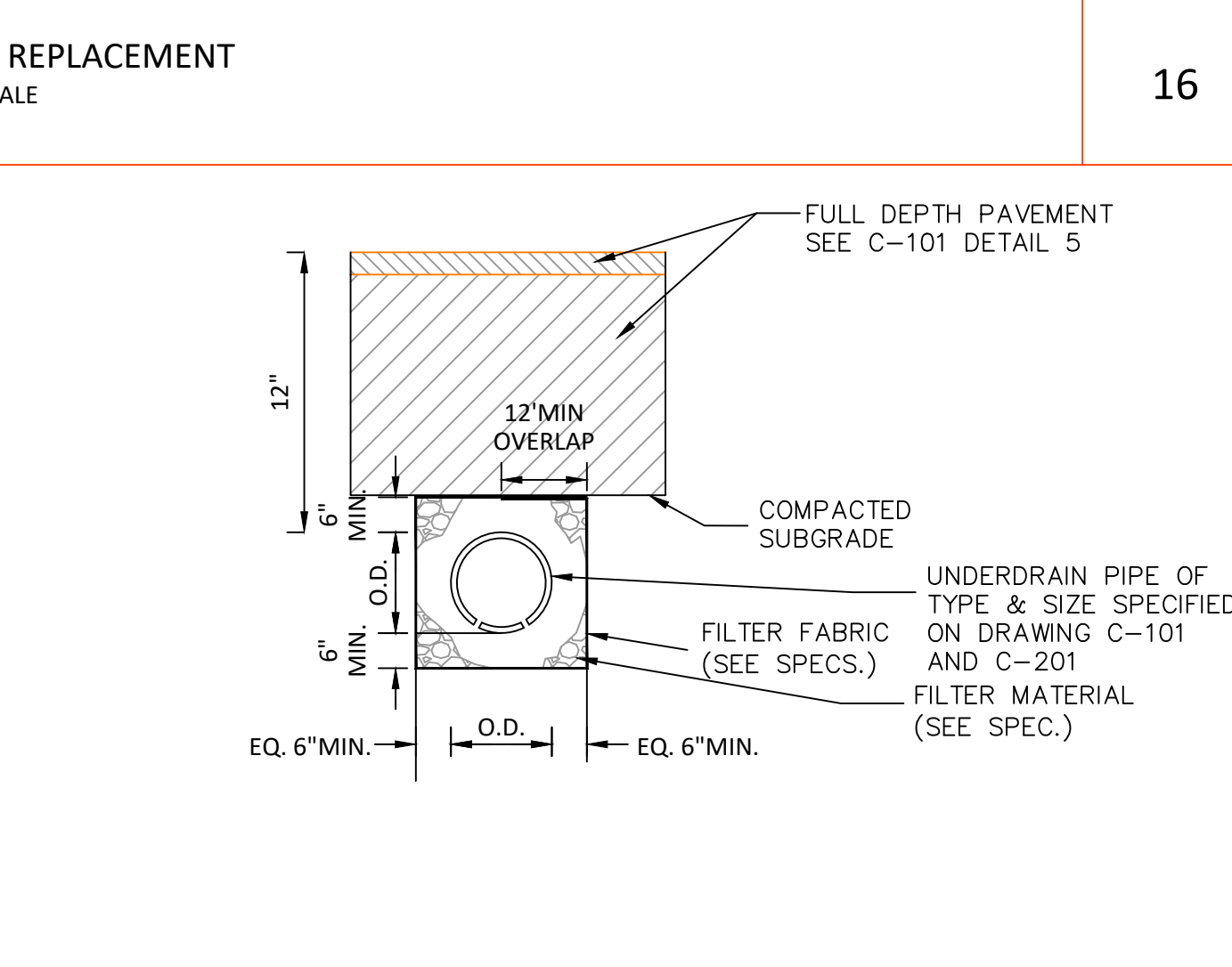
4 FOOT HIGH CHAIN LINK FENCE  
NOT TO SCALE

17



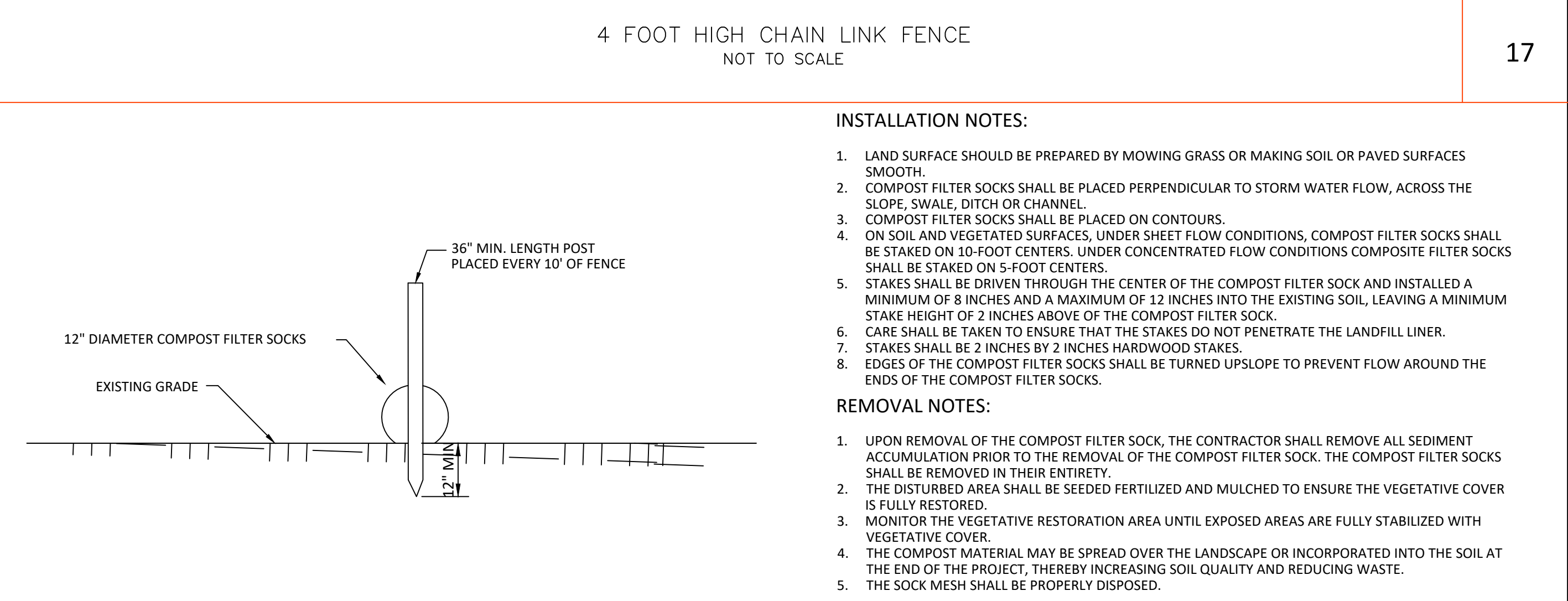
MODEL HPL - HELIPORT SYSTEMS, INC  
NOT TO SCALE

18



UNDERDRAIN INSTALLATION PAVED AREAS  
NOT TO SCALE

19



COMPOST FILTER SOCK  
NOT TO SCALE

20

| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS   | 06/22/2023 |
| 3   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 4   | PERMIT SET                    | 09/06/2023 |

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OGS PROJECT NUMBER SE391

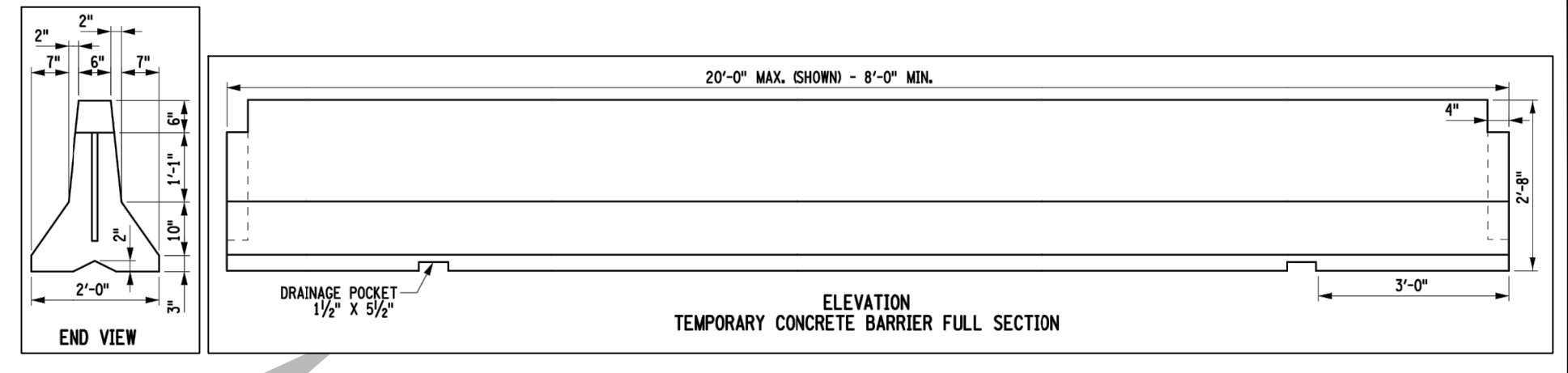
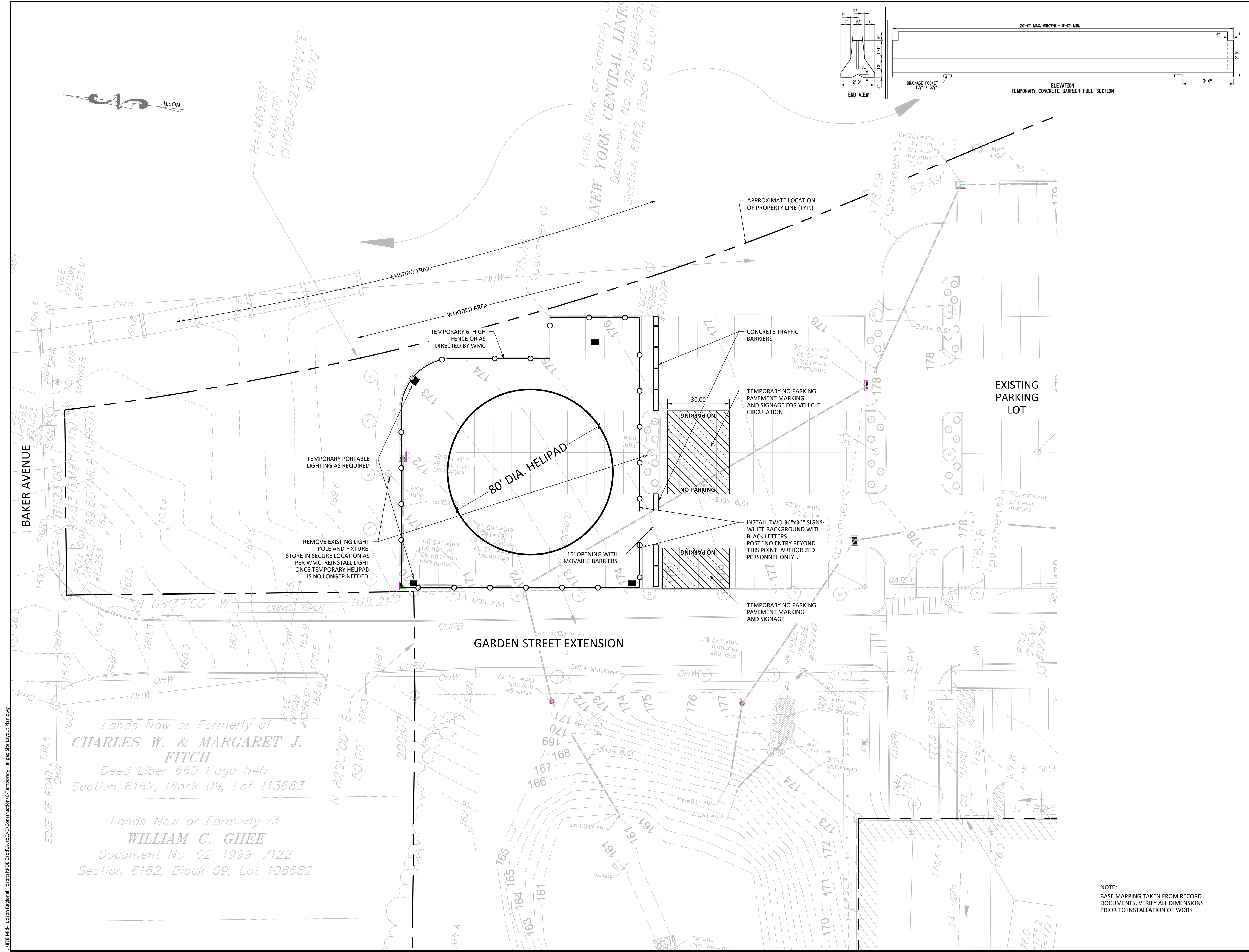
**TITLE: DETAILS**

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| Scale:       | N/A      |
| Date:        | 07/27/22 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 12 of 22 |
| Dwg. No.:    | C-602    |

EXPIRES 1/31/24

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| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | REVISIONS PER OGS COMMENTS    | 03/08/2023 |
| 2   | SITE PLAN SUBMISSION          | 04/24/2023 |
| 3   | REVISIONS PER TOWN COMMENTS   | 06/22/2023 |
| 4   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 5   | PERMIT SET                    | 09/06/2023 |

**CONSTRUCTION PLAN LEGEND**

- PROPOSED PROPERTY LINE
- PROPOSED EASEMENT LINE
- PROPOSED ROAD BASELINE
- EXISTING CURB
- PROPOSED CONC. CURB
- PROPOSED EDGE OF PAVEMENT
- [Pattern] PROPOSED CONCRETE
- [Pattern] PROPOSED MILL AND OVERLAY
- [Pattern] NYS DOT PAVEMENT SECTION
- [Pattern] PROPOSED ASPHALT
- [Pattern] PROPOSED CONCRETE PAD
- [Pattern] PROPOSED HELIPAD
- PROPOSED DROP CURB
- LIMIT OF DISTURBANCE
- 210 PROPOSED CONTOUR LINE
- x 210.5 PROPOSED SPOT ELEVATION



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241 North Road  
Poughkeepsie (T), New York  
WMC CONTRACT NUMBER - WMC-3829  
OGS PROJECT NUMBER SE391

**TITLE:**  
TEMPORARY HELIPAD  
SITE LAYOUT PLAN

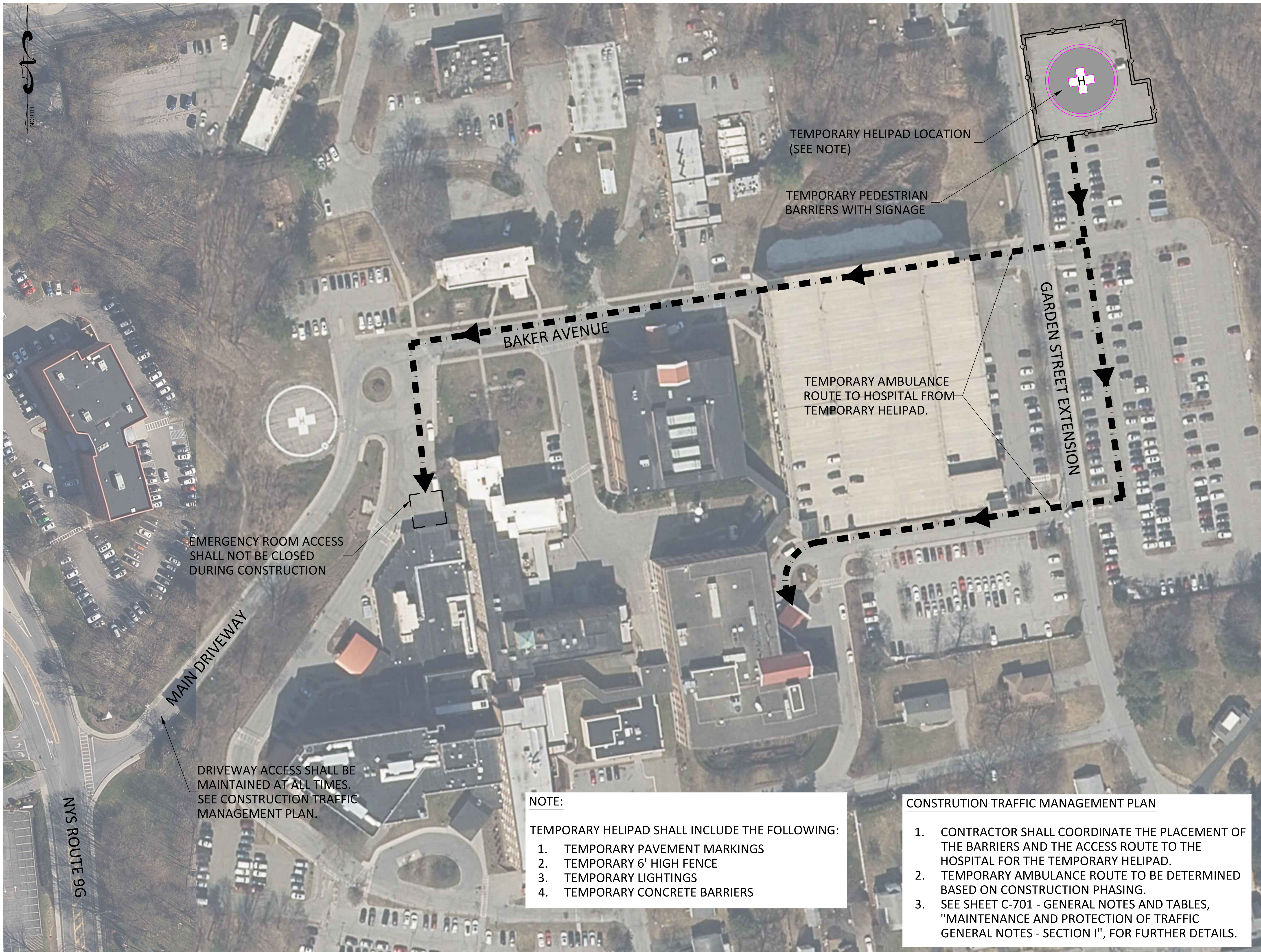
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| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 3 of 22  |
| Dwg. No.:    | C-801    |

EXPIRES 1/31/24

**NOTE:**  
BASE MAPPING TAKEN FROM RECORD DOCUMENTS. VERIFY ALL DIMENSIONS PRIOR TO INSTALLATION OF WORK

L:\2023\Mid-Hudson Regional Hospital\HDP\Construction\CAD\AutoCAD\Construction\Temporary Helipad Site Layout Plan.dwg

L:\278 Mid-Hudson Regional Hospital\PE\_Cad\AutoCAD\Construction\CT-101 Construction Traffic Plan.dwg



| NO. | REVISION                      | DATE       |
|-----|-------------------------------|------------|
| 1   | REVISIONS PER AGENCY COMMENTS | 08/11/2023 |
| 2   | PERMIT SET                    | 09/06/2023 |
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**EROSION CONTROL PLAN LEGEND**

- TEMPORARY FENCE/BARRIER
- - - HELIPAD/EMERGENCY ROOM AREA
- ← - - - TRAFFIC FLOW



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**Mid-Hudson Regional Hospital**  
 241 North Road  
 Poughkeepsie (T), New York  
 WMC CONTRACT NUMBER - WMC-3829  
 OGS PROJECT NUMBER SE391

TITLE:  
**CONSTRUCTION TRAFFIC MANAGEMENT PLAN**

|              |          |
|--------------|----------|
| Scale:       | 1"=50'   |
| Date:        | 04/24/23 |
| Drawn By:    | DC       |
| Checked By:  | GMS      |
| Project No.: | 0878     |
| Sheet No.:   | 14 of 22 |
| Dwg. No.:    | CT-101   |

EXPIRES 1/31/24

- NOTE:**
- TEMPORARY HELIPAD SHALL INCLUDE THE FOLLOWING:
1. TEMPORARY PAVEMENT MARKINGS
  2. TEMPORARY 6' HIGH FENCE
  3. TEMPORARY LIGHTINGS
  4. TEMPORARY CONCRETE BARRIERS

- CONSTRUCTION TRAFFIC MANAGEMENT PLAN**
1. CONTRACTOR SHALL COORDINATE THE PLACEMENT OF THE BARRIERS AND THE ACCESS ROUTE TO THE HOSPITAL FOR THE TEMPORARY HELIPAD.
  2. TEMPORARY AMBULANCE ROUTE TO BE DETERMINED BASED ON CONSTRUCTION PHASING.
  3. SEE SHEET C-701 - GENERAL NOTES AND TABLES, "MAINTENANCE AND PROTECTION OF TRAFFIC GENERAL NOTES - SECTION I", FOR FURTHER DETAILS.



SYMBOLS AND ABBREVIATIONS

| SYMBOL | ABBREVIATION                  | DESCRIPTION                                                 | SYMBOL | ABBREVIATION                         | DESCRIPTION                                   |
|--------|-------------------------------|-------------------------------------------------------------|--------|--------------------------------------|-----------------------------------------------|
|        | -                             | CONDUIT AND WIRING                                          |        | -                                    | FUSED SWITCH                                  |
|        | -                             | CONDUIT & WIRING TO BE REMOVED UON                          |        | GEN                                  | GENERATOR                                     |
|        | -                             | BURIED CONDUIT                                              |        | -                                    | GENERATOR RECEPTACLE                          |
|        | -                             | OVERHEAD CONDUCTORS                                         |        | GND                                  | GROUND AS PER LOCAL CODE                      |
|        | -                             | HOMERUN TO PANEL, ARROWS INDICATE # 1P                      |        | -                                    | GROUND BAR                                    |
|        | -                             | MULTI-POLE HOMERUN                                          |        | -                                    | GROUND ROD                                    |
|        | -                             | ELECTRICAL EQUIPMENT AS INDICATED                           |        | -                                    | TRANSFER SWITCH                               |
|        | -                             | ELECTRICAL EQUIPMENT TO BE REMOVED UON                      |        | XFMR                                 | TRANSFORMER                                   |
|        | -                             | ELECTRIC METER                                              |        | CT                                   | CURRENT TRANSFORMER                           |
|        | -                             | JUNCTION BOX                                                |        | -                                    | UTILITY POLE                                  |
|        | -                             | FUSED DISCONNECT SWITCH                                     |        | WM                                   | WATER MAIN                                    |
|        | -                             | UNFUSED DISCONNECT SWITCH                                   |        | -                                    | BOILER BREAK GLASS STATION                    |
|        | -                             | COMBINATION MOTOR STARTER/FUSED DISC.                       |        | NC                                   | NORMALLY CLOSED CONTACTS                      |
|        | -                             | MOTOR STARTER                                               |        | NO                                   | NORMALLY OPEN CONTACTS                        |
|        | -                             | MOTOR                                                       |        | CV                                   | CONTROL VALVE                                 |
|        | -                             | BATTERY PACK EMERGENCY LIGHT FIXTURE                        |        | -                                    | LEAK DETECTOR                                 |
|        | -                             | EXIT LIGHT, FACES-SHADED, CHEVRON-ARROW                     |        | MD                                   | MOTORIZED DAMPER                              |
|        | S <sub>x</sub>                | SINGLE POLE SWITCH (x - INDICATES FIXTURE BEING CONTROLLED) |        | SD OR CFSD                           | SMOKE DAMPER                                  |
|        | S <sub>x</sub> <sup>3</sup>   | THREE WAY SWITCH (x - INDICATES FIXTURE BEING CONTROLLED)   |        | UH                                   | UNIT HEATER                                   |
|        | S <sub>x</sub> <sup>4</sup>   | FOUR WAY SWITCH (x - INDICATES FIXTURE BEING CONTROLLED)    | -      | -                                    | SURFACE MOUNTED RACEWAY, DEVICES AS INDICATED |
|        | S <sub>x</sub> <sup>DIM</sup> | DIMMER SWITCH (x - INDICATES FIXTURE BEING CONTROLLED)      | A      | AMPERE(S)                            |                                               |
|        | S <sub>M</sub>                | MOTOR RATED TOGGLE SWITCH                                   | AC     | AIR CONDITIONER                      |                                               |
|        | S <sub>K</sub>                | KEY OPERATED SINGLE POLE SWITCH                             | ACC    | AIR CONDITIONER CONDENSER            |                                               |
|        | S <sub>V</sub>                | SPEED CONTROLLER (FB0)                                      | AFF    | ABOVE FINISHED FLOOR                 |                                               |
|        | -                             | WALL MOUNTED OCCUPANCY SENSOR                               | AF     | AMPERAGE OF FUSE                     |                                               |
|        | -                             | CEILING MOUNTED OCCUPANCY SENSOR                            | AGL    | ABOVE GRADE LEVEL                    |                                               |
|        | -                             | DUPLEX RECEPTACLE                                           | AHU    | AIR HANDLING UNIT                    |                                               |
|        | -                             | DOUBLE DUPLEX RECEPTACLE                                    | AL     | ALUMINIUM                            |                                               |
|        | -                             | SPECIAL RECEPTACLE                                          | ARC    | ARC FAULT INTERRUPTER                |                                               |
|        | -                             | TELEPHONE OUTLET                                            | AS     | AMPERAGE OF SWITCH                   |                                               |
|        | ▼ <sup>x</sup>                | DATA OUTLET (x - INDICATES # OF JACKS, 1 JACK UON)          | ATS    | AUTOMATIC TRANSFER SWITCH            |                                               |
|        | -                             | SECURITY ALARM HORN                                         | AWG    | AMERICAN WIRE GAUGE                  |                                               |
|        | -                             | PUSHBUTTON                                                  | BCW    | BARE COPPER WIRE                     |                                               |
|        | -                             | CALL FOR AID PULL STATION                                   | BLDG   | BUILDING                             |                                               |
|        | -                             | SECURITY CAMERA PTZ - PAN, TILT, ZOOM                       | BMS    | BUILDING MANAGEMENT SYSTEM           |                                               |
|        | -                             | CALL FOR AID A/V DEVICE                                     | C      | CONDUIT                              |                                               |
|        | EPO                           | EMERGENCY POWER OFF SWITCH                                  | CD     | CANDELA                              |                                               |
|        | RASP                          | RESCUE ASSIST. SYSTEM MASTER STATION                        | CKT    | CIRCUIT                              |                                               |
|        | -                             | REMOTE RESCUE STATION                                       | CLG    | CEILING                              |                                               |
|        | -                             | PUBLIC ADDRESS BELL                                         | COL    | COLUMN                               |                                               |
|        | -                             | CLOCK                                                       | CU     | COPPER                               |                                               |
|        | -                             | TIME CLOCK                                                  | CUH    | CABINET UNIT HEATER                  |                                               |
|        | -                             | CARD READER                                                 | CUH    | CABINET UNIT HEATER                  |                                               |
|        | -                             | DOOR ALARM                                                  | DEM.   | DEMOLISH AND REMOVE                  |                                               |
|        | -                             | ELECTRIC DOOR STRIKE                                        | DISC   | DISCONNECT                           |                                               |
|        | -                             | SECURITY MOTION DETECTOR                                    | DIM    | DIMMER                               |                                               |
|        | -                             | KEY PAD                                                     | DWG    | DRAWING                              |                                               |
|        | -                             | PANIC ALARM                                                 | ELEV   | ELEVATOR                             |                                               |
|        | EPO                           | EMERGENCY POWER OFF SWITCH                                  | EMT    | ELECTRICAL METALLIC TUBING           |                                               |
|        | RASP                          | RESCUE ASSIST. SYSTEM MASTER STATION                        | EM     | EMERGENCY                            |                                               |
|        | -                             | REMOTE RESCUE STATION                                       | EX.    | EXISTING TO REMAIN                   |                                               |
|        | CB                            | CIRCUIT BREAKER                                             | F      | FLOOR                                |                                               |
|        | -                             | ENCLOSED CIRCUIT BREAKER                                    | FBO    | FURNISHED BY OTHERS                  |                                               |
|        |                               |                                                             | FC     | FAN COIL UNIT                        |                                               |
|        |                               |                                                             | GEN    | GENERATOR                            |                                               |
|        |                               |                                                             | GFI    | GROUND FAULT INTERRUPTER             |                                               |
|        |                               |                                                             | HP     | HORSEPOWER                           |                                               |
|        |                               |                                                             | HVAC   | HEATING VENTILATION AIR CONDITIONING |                                               |
|        |                               |                                                             | IG     | ISOLATED GROUND                      |                                               |
|        |                               |                                                             | IMC    | INTERMEDIATE METAL CONDUIT           |                                               |

NOTES:  
 1.) ALL SYMBOLS AND ABBREVIATIONS MAY NOT BE APPLICABLE FOR THIS PROJECT.  
 2.) SEE LIGHTING FIXTURE SCHEDULE FOR LIGHT FIXTURE SYMBOLS.

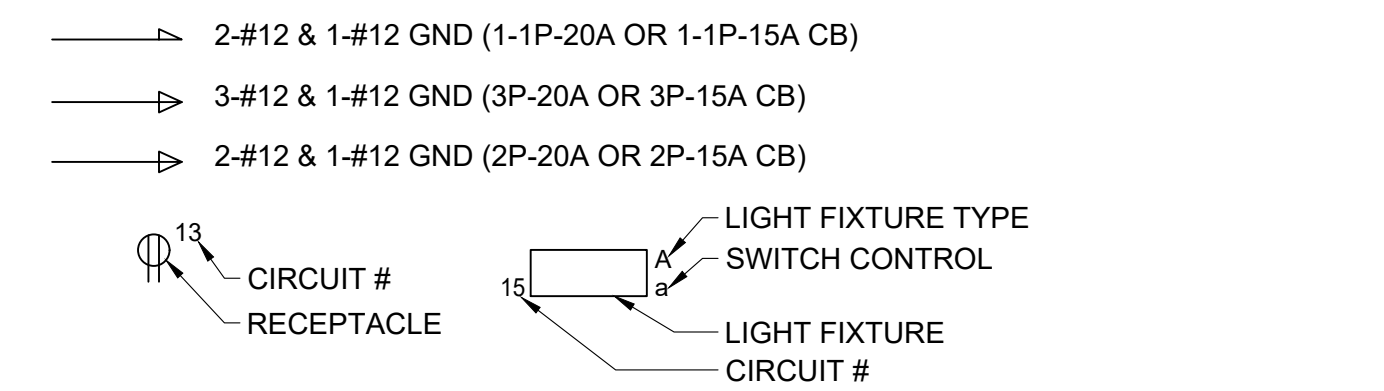
GENERAL NOTES

- ALL WORK SHOWN IS NEW UNLESS OTHERWISE NOTED (UON) EXISTING TO REMAIN (EX.).
- THE DRAWINGS ARE TO BE CONSIDERED SCHEMATIC ONLY AND DO NOT NECESSARILY SHOW THE EXACT LOCATIONS AND DETAILS OF THE WORK TO BE INSTALLED.
- THE ELECTRICAL CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND PAYING ALL FEES ASSOCIATED WITH THIS WORK INCLUDING FILING WITH THE UTILITY COMPANY (AS REQUIRED), AND WITH LOCAL AUTHORITY HAVING JURISDICTION.
- ELECTRICAL CONTRACTOR SHALL BE RESPONSIBLE TO HIRE A THIRD PARTY ELECTRICAL INSPECTION AGENCY TO PROVIDE UL INSPECTIONS AND SUBMIT A CERTIFICATE OF INSPECTION PRIOR TO FINAL REQUEST FOR PAYMENT.
- ALL WORK INVOLVING THE ELECTRIC SERVICE SHALL BE COORDINATED AND APPROVED BY THE UTILITY COMPANY.
- ALL CONDUCTORS SHALL BE COPPER UON ON DRAWINGS.
- ELECTRONIC FILES OF THE ELECTRICAL DRAWINGS ARE AVAILABLE TO THE CONTRACTOR. THE ENGINEER MAY GRANT THE CONTRACTOR A LIMITED LICENSE TO MAKE A DERIVATIVE WORK OF THE DATABASE FOR THE PURPOSE OF SHOP DRAWINGS, SUBMITTALS AND AS-BUILT DRAWINGS. UPON REQUEST, THE ENGINEER SHALL PROVIDE A RELEASE FORM THAT MUST BE SIGNED AND RETURNED BY THE CONTRACTOR PRIOR TO RELEASE OF THE ELECTRONIC FILES.
- CIRCUIT NUMBERS ARE FOR INFORMATION PURPOSES ONLY. ACTUAL CIRCUIT NUMBERS SHALL BE DETERMINED IN THE FIELD.
- INSTALL CONDUIT EXPANSION FITTINGS AT ALL LOCATIONS WHERE CONDUITS CROSS BUILDING OR STRUCTURE EXPANSION JOINTS.
- THE ELECTRICAL CONTRACTOR SHALL PROVIDE ALL CUTTING, PATCHING, PAINTING, AND FINAL RESTORATION REQUIRED TO FACILITATE THE DEMOLITION AND INSTALLATION OF ALL ELECTRICAL EQUIPMENT, INCLUDING BUT NOT LIMITED TO PANELBOARDS, CONDUITS, WIRING, DEVICES, FIXTURES, ETC. INCLUDING ABOVE CEILINGS. CONTRACTOR TO REMOVE AND REPLACE CEILINGS, AND OPEN AND PATCH WALLS, AS REQUIRED TO EXECUTE THE ELECTRICAL WORK.

DEFINITION OF TERMS

- WHEREVER IN THE CONTRACT DOCUMENTS THE WORD "CLIENT" IS USED, IT MUST BE UNDERSTOOD THAT "MID-HUDSON REGIONAL HOSPITAL" IS INTENDED.
- WHEREVER IN THE CONTRACT DOCUMENTS THE WORD "SITE ENGINEER" IS USED, IT MUST BE UNDERSTOOD THAT "DTS PROVIDENT" IS INTENDED.
- WHEREVER IN THE CONTRACT DOCUMENTS THE WORD "ENGINEER" IS USED, IT MUST BE UNDERSTOOD THAT "OLA CONSULTING ENGINEERS" IS INTENDED.
- WHEREVER IN THE CONTRACT DOCUMENTS THE WORDS "ELECTRICAL UTILITY" OR "POWER COMPANY" ARE USED, IT MUST BE UNDERSTOOD THAT "CENTRAL HUDSON GAS & ELECTRIC CORP" IS INTENDED.
- "WORK" MUST BE DEEMED TO CONSIST OF ALL LABOR AND OPERATIONS, TRANSPORTATION, HOISTING, MATERIALS, TOOLS, EQUIPMENT, SERVICES, INSPECTIONS, INVESTIGATIONS, COORDINATION AND SUPERVISION REQUIRED AND / OR REASONABLY NECESSARY TO PRODUCE THE CONSTRUCTION REQUIRED BY THE CONTRACT DOCUMENTS.
- "FURNISH" MEANS THE DESIGN, FABRICATION, PURCHASE AND DELIVERY TO THE JOB SITE.
- "INSTALL OR INSTALLATION" MEANS THE ACT OF PHYSICALLY PLACING, APPLYING, SETTING, ERECTING, ANCHORING, SECURING, ETC. CONSTRUCTION MATERIALS, EQUIPMENT, FURNISHINGS, APPLIANCES, AND SIMILAR ITEMS SPECIFIED AND FURNISHED AT THE JOB SITE. INSTALLATION OF SPECIFIED ITEMS MUST BE COMPLETE IN ALL RESPECTS.
- "PROVIDE" MEANS TO FURNISH AND INSTALL CONSTRUCTION MATERIAL, EQUIPMENT, ETC. AS DEFINED ABOVE.
- THE FOLLOWING ARE DEFINITIONS OF SHOP DRAWING STAMP ACTIONS:
  - "NO EXCEPTIONS TAKEN" MEANS THAT THE SHOP DRAWING IS CORRECT AS TO PERFORMANCE, CAPACITY, ETC. AND SUBSTANTIAL CONFORMANCE TO THE CONTRACT DRAWINGS AND SPECIFICATIONS. FABRICATION AND/OR PURCHASE MAY COMMENCE.
  - "MAKE CORRECTIONS NOTED" MEANS THAT THE SHOP DRAWING IS CORRECT AS TO PERFORMANCE, CAPACITY, ETC. AND SUBSTANTIAL CONFORMANCE TO THE CONTRACT DRAWINGS AND/OR SPECIFICATIONS, SUBJECT TO AND IN COMPLIANCE WITH THE ANNOTATIONS AND/OR CORRECTIONS INDICATED ON THE SHOP DRAWING. FABRICATION AND/OR PURCHASE MAY COMMENCE.
  - "AMEND AND RESUBMIT" MEANS THAT THE COMMENTS AND/OR CORRECTION ARE SO EXTENSIVE AND IMPORTANT THAT THE REVIEWER WANTS TO SEE HOW THE COMMENTS AND/OR CORRECTIONS ARE RESOLVED PRIOR TO RELEASE FOR FABRICATION AND/OR PURCHASE. FABRICATIONS AND/OR PURCHASE MAY NOT COMMENCE.
  - "REJECTED" MEANS THAT THE SHOP DRAWING DOES NOT COMPLY OR CONFORM TO THE CONTRACT DRAWINGS AND/OR SPECIFICATIONS. FABRICATION AND/OR PURCHASE MAY NOT COMMENCE.

TYPICAL BRANCH CIRCUIT WIRING LEGEND



- NOTES:  
 1. EACH 120V AND 277V CIRCUIT SHALL HAVE A DEDICATED NEUTRAL CONDUCTOR. SHARED NEUTRAL HOMERUNS ARE NOT PERMITTED.  
 2. CONDUCTORS SHALL BE INCREASED FOR VOLTAGE DROP AND DERATING AS PER APPLICABLE ELECTRICAL CODE. FOR CIRCUITS THAT ARE BETWEEN 100' AND 150' IN LENGTH, PHASE AND NEUTRAL CONDUCTORS SHALL BE #10 AWG. FOR CIRCUITS THAT ARE BETWEEN 150' AND 225' IN LENGTH, PHASE AND NEUTRAL CONDUCTORS SHALL BE #8 AWG. FOR LENGTHS GREATER THAN 225' IN LENGTH, VERIFY CONDUCTOR SIZES WITH ENGINEER.

APPLICABLE CODES

- ALL WORK SHALL CONFORM WITH CURRENT CODES, INCLUDING:
- 2020 NEW YORK STATE BUILDING CODE
  - 2020 NEW YORK STATE ENERGY CONSERVATION CODE
  - 2017 NATIONAL ELECTRIC CODE

TRENCHING NOTES

- CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES THAT ARE NOT PART OF N.Y. STATE "CODE 753" PRIOR TO DIGGING.
- ALL EXCAVATING IN THE AREA OF THE EXISTING UNDERGROUND EQUIPMENT, PIPES AND CONDUITS SHALL BE PERFORMED BY HAND.
- ANY AREA/PLANTS OR LANDSCAPING OR PAVEMENTS DISTURBED DURING THE EXCAVATION SHALL BE RESTORED OR REPLACED TO MATCH EXISTING CONDITIONS BY THE CONTRACTOR AT NO COST TO THE OWNER.
- ANY EXISTING BURIED CONDUITS, DRAINAGE, SPRINKLER PIPING, ETC. THAT IS DISTURBED AND/OR DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE OWNER.
- THE PLANS SHOW SOME KNOWN SUBSURFACE STRUCTURES, ABOVE GROUND STRUCTURES AND/OR UTILITIES BELIEVED TO EXIST IN THE WORKING AREA, EXACT LOCATION OF WHICH MAY VARY FROM THE LOCATIONS INDICATED. IN PARTICULAR, THE CONTRACTOR IS WARNED THAT THE EXACT OR EVEN APPROXIMATE LOCATION OF SUCH PIPELINES, SUBSURFACE STRUCTURES AND/OR UTILITIES IN THE AREA MAY OR MAY NOT BE SHOWN; AND IT SHALL BE HIS RESPONSIBILITY TO PROCEED WITH GREAT CARE IN EXECUTING ANY WORK. 48 HOURS BEFORE YOU DIG, DRILL OR BLAST, CALL 1-800-272-4480 (NYC & LI) OR 1-800-962-7962 (NY STATE).

| NO. | REVISION                          | DATE       |
|-----|-----------------------------------|------------|
| 1   | SITE PLAN APPLICATION             | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS       | 06/22/2023 |
| 3   | ISSUED FOR REVIEW                 | 08/18/2023 |
| 4   | REVISED PER TOWN AND OGS COMMENTS | 08/23/2023 |

MID-HUDSON REGIONAL HOSPITAL  
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TITLE: ELECTRICAL SYMBOLS, ABBREVIATIONS AND GENERAL NOTES

Scale:  
 Date: 04/24/2023  
 Drawn By: JV  
 Checked By: JL  
 Project No.: NDT50004.00  
 Sheet No.: 1 of 7  
 Dwg. No.: E001

EXP:08/23/2023



SPECIFICATIONS CONTINUED

- BACKBOXES, APPROVED HANGERS, ALL REQUIRED MISCELLANEOUS HARDWARE AND LAMPS. ALL STEEL PARTS SHALL BE BONDERIZED AND PHOSPHATIZED. ALL FIXTURES AND TRIMS SHALL BE FREE FROM LIGHT LEAKS.
2. SUPPORT EACH FIXTURE SECURELY. RECESSED FLUORESCENT FIXTURES SHALL BE SECURED AT A MINIMUM OF TWO POINTS TO THE BUILDING STRUCTURE TO MEET THE LOCAL BUILDING CODE SEISMIC REQUIREMENTS.
3. UPON COMPLETION OF WORK AND AFTER THE BUILDING AREA IS BROOM CLEAN, ALL FIXTURES SHALL BE MADE CLEAN. USE DESTAINTIZING CLOTH ON ALL PLASTIC AND GLASS MATERIAL.
4. RELAMPING ACCESS SHALL REQUIRE NO SPECIAL TOOLS.
5. ALL FLUORESCENT FIXTURES SHALL BE EQUIPPED WITH ELECTRONIC BALLASTS.
6. ALL FLUORESCENT LAMPS SHALL BE WARM WHITE 3000K, 82 CRI UNLESS OTHERWISE INDICATED. FURNISH NEW LAMPS FOR ALL NEW FIXTURES.

I. NEW CIRCUIT BREAKER PANELBOARDS:

- 1. FURNISH AND INSTALL CIRCUIT BREAKER PANELBOARDS AS INDICATED ON THE DRAWINGS. PANELBOARDS SHALL BE DEAD FRONT SAFETY TYPE EQUIPPED WITH THERMAL-MAGNETIC, BOLTED TYPE, MOLDED CASE CIRCUIT BREAKERS OF FRAME AND TRIP RATINGS AS SHOWN ON THE DRAWINGS. PANELBOARD BUS STRUCTURE AND MAIN LUGS OR MAIN BREAKER SHALL HAVE CURRENT RATINGS AS DRAWINGS. ALL BUSBARS SHALL BE COPPER. ALL PANELBOARDS SHALL BE SEISMIC RATED ACCORDING TO THE LOCAL BUILDING CODE REQUIREMENTS.
2. CIRCUIT BREAKERS SHALL BE EQUIPPED WITH INDIVIDUALLY INSULATED, BRACED AND PROTECTED CONNECTORS. THE FRONT FACES OF ALL CIRCUIT BREAKERS SHALL BE FLUSH WITH EACH OTHER. LARGE PERMANENT INDIVIDUAL CIRCUIT NUMBERS SHALL BE AFFIXED TO EACH BREAKER IN A UNIFORM POSITION. TRIPPED INDICATION SHALL BE CLEARLY SHOWN BY THE BREAKER HANDLE TAKING A POSITION BETWEEN ON AND OFF. PROVISIONS FOR ADDITIONAL BREAKERS SHALL BE SUCH THAT NO ADDITIONAL CONNECTORS WILL BE REQUIRED TO ADD BREAKERS.
3. EACH PANELBOARD, AS A COMPLETE UNIT, SHALL HAVE A RATING EQUAL TO OR GREATER THAN THE INTEGRATED EQUIPMENT RATING SHOWN ON THE DRAWINGS. PANELBOARD ASSEMBLY SHALL BE ENCLOSED IN A STEEL CABINET. THE RIGIDITY AND GAUGE OF STEEL TO BE AS SPECIFIED IN UL STANDARD 50 FOR CABINETS. THE SIZE OF WIRING GUTTERS SHALL BE IN ACCORDANCE WITH UL STANDARD 67 FOR PANELBOARDS. FRONTS SHALL INCLUDE DOORS AND HAVE FLUSH, BRUSHED STAINLESS STEEL, CYLINDER TUMBLER-TYPE LOCKS WITH CATCHES AND SPRING-LOADED DOOR PULLS. THE FLUSH LOCK SHALL NOT PROTRUDE BEYOND THE FRONT OF THE DOOR. ALL PANELBOARD LOCKS SHALL BE KEYED ALIKE. FRONT SHALL HAVE ADJUSTABLE INDICATING TRIM CLAMPS WHICH SHALL BE COMPLETELY CONCEALED WHEN THE DOORS ARE CLOSED. DOORS SHALL BE MOUNTED BY COMPLETELY CONCEALED STEEL HINGES. FRONTS SHALL NOT BE REMOVABLE WITH DOOR IN THE LOCKED POSITION. A CIRCUIT DIRECTORY FRAME AND CARD WITH A CLEAR PLASTIC COVERING SHALL BE PROVIDED ON THE INSIDE OF THE DOOR. THE DIRECTORY CARD SHALL PROVIDE A SPACE AT LEAST 1/4" HIGH AND 3" LONG OR EQUIVALENT FOR EACH CIRCUIT. THE DIRECTORY SHALL BE TYPED TO IDENTIFY THE LOAD FED BY EACH CIRCUIT. FRONTS SHALL BE OF CODE GAUGE, FULL FINISHED STEEL WITH RUST-INHIBITING PRIMER AND BAKED ENAMEL FINISH.
4. THE PANELBOARD INTERIOR ASSEMBLY SHALL BE DEAD FRONT WITH PANELBOARD FRONT REMOVED. MAIN LUGS OR MAIN BREAKER SHALL BE BARRIERED ON FIVE SIDES. THE BARRIER IN FRONT OF THE MAIN LUGS SHALL BE HINGED TO A FIXED PART OF THE INTERIOR. THE END OF THE BUS STRUCTURE OPPOSITE THE MAINS SHALL BE BARRIERED.
5. 208/120 VOLT PANELBOARDS SHALL BE PROVIDED WITH FACTORY INSTALLED 100% RATED NEUTRAL BUS AND GROUND BUS WHICH SHALL HAVE PROVISIONS FOR EACH CIRCUIT IN THE PANELBOARD. EQUIPMENT GROUND BUS SHALL BE BRAZED TO PANELBOARD ENCLOSURE.
6. PANELBOARDS SHALL BE LISTED BY UNDERWRITERS' LABORATORIES AND SHALL BEAR THE UL LABEL.

J. DISCONNECT SWITCHES:

- 1. THE CONTRACTOR SHALL FURNISH AND INSTALL FUSIBLE OR NON-FUSIBLE DISCONNECT SWITCHES AS REQUIRED AND/OR SHOWN ON THE DRAWINGS.
2. THE DISCONNECT SWITCHES, UNLESS OTHERWISE INDICATED OR SPECIFIED, SHALL BE HEAVY-DUTY, QUICK-MAKE, QUICK-BREAK OPERATED, IN NEMA 1 OR 3R ENCLOSURES, OF A CAPACITY, TYPE AND NUMBER OF POLES AS NOTED ON THE DRAWINGS. THE MAIN LUGS SHALL BE ADEQUATE TO ACCEPT THE SIZES OF CABLE INDICATED ON THE DRAWINGS.
3. ALL DISCONNECT SWITCHES SHALL BE FRONT OPERATED AND EACH SHALL CONTAIN A GROUNDING LUG WELDED TO THE INSIDE OF THE SWITCH ENCLOSURE.
4. SWITCHES SHALL BE HORSEPOWER RATED FOR LOAD SERVED AND RATED FOR 200KAIC RMS SYMMETRICAL FAULT CURRENT.
5. SWITCHES SHALL HAVE PROVISIONS TO BE LOCKED IN THE OPEN POSITION WITH CLIPS TO ACCEPT CLASS J FUSES.
6. SWITCHES SHALL MEET NEMA STANDARD KS-1-1990 FOR TYPE HD SWITCHES AND SHALL BE U.L. LISTED.

N. PULLBOXES AND TROUGHS:

- 1. PULLBOXES AND TROUGHS WITH COVERS SHALL BE FABRICATED FROM MINIMUM #12 USSG GALVANIZED SHEET STEEL WITH ALL SEAMS AND JOINTS WELDED AND GROUND SMOOTH. COVERS SHALL BE SECURED TO PULLBOXES WITH NICKEL OR CADMIUM PLATED, OVAL HEAD SCREWS PROVIDED WITH STOP BEAD WASHERS. TROUGHS SHALL HAVE HINGED COVERS AND SHALL BE HELD CLOSED WITH EXTERNAL CLAMPS. DIMENSIONS OF BOXES AND TROUGHS SHALL BE AS REQUIRED BY ARRANGEMENT OF CONDUITS, EQUIPMENT OR APPLICABLE CODE REQUIREMENTS.
2. PULLBOXES AND TROUGHS SHALL BE FINISHED INSIDE AND OUTSIDE WITH A SHOP-APPLIED COAT OF ASA #61 LIGHT GRAY ENAMEL.
3. THE CONTRACTOR SHALL PROVIDE ALL PULLBOXES REQUIRED TO PULL WIRES IN CONDUIT RUNS WHETHER INDICATED ON THE DRAWINGS OR NOT. BOXES AND TROUGHS USING CONCENTRIC OR ACENTRIC KNOCKOUTS SHALL BE GROUNDED TO THE INCOMING CONDUITS BY MEANS OF GROUNDING FITTINGS AND BONDING JUMPERS. OZ TYPE BLG INSULATED GROUNDING BUSHINGS, AS SPECIFIED ELSEWHERE, SHALL BE USED. BONDING JUMPERS SHALL BE COPPER SIZED IN ACCORDANCE WITH THE AFOREMENTIONED CODE. A GROUND LUG SHALL BE WELDED INSIDE EACH BOX AND TROUGH.

O. ALTERATIONS AND REMOVAL OF EXISTING EQUIPMENT, CONDUIT & WIRING:

- 1. THE EXISTING BUILDING ELECTRICAL SYSTEMS SHALL BE MAINTAINED IN OPERATION DURING THE CONSTRUCTION PERIOD. EXISTING SYSTEMS SHALL NOT BE SHUT DOWN NOR SHALL CONNECTIONS BE MADE THERETO WITHOUT PRIOR APPROVAL OF THE OWNER.
2. CERTAIN EXISTING CONDUITS AND ASSOCIATED WIRING ARE INDICATED ON THE DRAWINGS ACCORDING TO THE BEST INFORMATION AVAILABLE. CERTAIN OTHER EXISTING CONDUITS AND ASSOCIATED WIRING MAY NOT BE SHOWN. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO DETERMINE THE LOCATION

OF EXISTING CONDUIT AND WIRING AS REQUIRED FOR NEW CONSTRUCTION OR IF DAMAGED DURING CUTTING OPERATIONS, REPLACE/REPAIR AT NO EXPENSE TO THE OWNER.

- 3. WHERE EQUIPMENT IS REMOVED OR WHERE WALLS AND CEILINGS ARE DEMOLISHED, WIRING DEVICES, CONDUIT, WIRING AND INSTALLATION MATERIAL (FITTINGS, BOXES, HANGERS, SUPPORTS, ETC.) THAT IS NOT TO BE REUSED SHALL BE REMOVED. ALL CONDUITS REMOVED SHALL BE CUT FLUSH WITH CONSTRUCTION AND OPENINGS PATCHED. ALL WIRING REMOVED SHALL BE DISCONNECTED AS FAR BACK AS THE BRANCH CIRCUIT PANELBOARD TERMINALS UNLESS OTHERWISE NOTED. WHERE WIRING IS TO REMAIN IN EXISTING CONDUITS TO MAINTAIN CONTINUITY OF CIRCUITS AND PASSES THROUGH OUTLET BOXES NOT TO BE REUSED FOR WIRING DEVICES OR LIGHTING FIXTURES, SUCH OUTLETS SHALL BE FURNISHED WITH COVERPLATES. ACTIVE CIRCUITS, IF REQUIRED AND NECESSARY TO REMAIN, SHALL BE REROUTED WITH NEW MATERIALS.
4. ALL EQUIPMENT WHICH IS BEING REMOVED AND NOT BEING REUSED SHALL BE RETURNED TO THE OWNER OR DISPOSED OF AS DIRECTED.
5. CONTRACTOR SHALL MEASURE STEADY STATE LOAD CURRENTS ON EACH PANELBOARD FEEDER OR EACH PANELBOARD THAT WAS ALTERED. SHOULD THE DIFFERENCE AT ANY PANELBOARD BETWEEN PHASES EXCEED 20 PERCENT, REARRANGE CIRCUITS IN PANELBOARD TO BALANCE THE PHASE LOAD WITHIN 20 PERCENT. TAKE CARE TO MAINTAIN PROPER PHASING FOR MULTI-WIRE BRANCH CIRCUITS. UPDATE DIRECTORIES ACCORDINGLY.

E-9 PAINTING

A. PULL BOXES AND WIREWAYS SHALL BE SHOP PAINTED INSIDE AND OUTSIDE WITH ONE COAT OF PRIMER AND ONE COAT OF ENAMEL UNDERCOATER IN A LIGHT GRAY COLOR AS APPROVED BY THE CLIENT'S REPRESENTATIVE.

E-10 IDENTIFICATION

A. THE CONTRACTOR SHALL PROVIDE UPDATED TYPE WRITTEN PANELBOARD DIRECTORIES IN ALL NEW PANELBOARDS AND ANY EXISTING PANELBOARD THAT HAS BEEN ALTERED. CONTRACTOR SHALL TRACE CIRCUITS TO REMAIN AS REQUIRED.

B. ALL ELECTRICAL EQUIPMENT, SUCH AS PANELS, AND ALL OTHER SIMILAR ITEMS WHICH ARE FURNISHED UNDER THIS HEADING OF THE SPECIFICATIONS SHALL BE ADEQUATELY IDENTIFIED WITH ENGRAVED LAMINATED PLASTIC NAMEPLATE HAVING BLACK BACKGROUNDS AND WHITE LETTERS. WORDING ON THE NAMEPLATES SHALL CLEARLY INDICATE THE NAMES AND FUNCTIONS OF THE EQUIPMENT. THE CONTRACTOR SHALL SUBMIT FOR APPROVAL, FIVE COPIES OF A LIST OF ALL EQUIPMENT TO BE IDENTIFIED TOGETHER WITH THE WORDING TO BE USED ON THE NAMEPLATES BEFORE ORDERING.

C. A MAINTENANCE LABEL SHALL BE AFFIXED TO ALL EQUIPMENT REQUIRING PREVENTATIVE MAINTENANCE. TWO COPIES OF ALL MAINTENANCE MANUALS SHALL BE PROVIDED TO THE CLIENT.

D. ALL FEEDERS SHALL BE TAGGED WITH APPROVED-TYPE STENCILED METAL TAGS IN ALL PANELS AND PULLBOXES THROUGH WHICH THEY ARE ROUTED. THIS TAGGING SHALL INCLUDE FEEDER NUMBER, PANEL SOURCE, CIRCUIT NUMBER, FEEDER SIZE AND EQUIPMENT SUPPLIED.

E. EACH DUPLEX AND QUAD RECEPTACLE SHALL BE LABELED WITH THE CIRCUIT NUMBER WHICH IT SERVES.

E-11 TESTING

A. ALL CIRCUITS SHALL BE TESTED FOR UNWANTED GROUNDS AND PROPER PHASE RELATION.

B. THE CONTRACTOR SHALL PROVIDE QUALIFIED PERSONNEL TO CONDUCT AND/OR TO ASSIST THE CLIENT'S REPRESENTATIVE TO CONDUCT OPERATING TESTS AT THE COMPLETION OF THE WORK. THESE OPERATING TESTS WILL INCLUDE CHECKING THE FOLLOWING ELECTRICAL SYSTEMS:

- 1. WIRING DEVICES: A CHECK OF RECEPTACLES SHALL BE CHECKED FOR SMOOTHNESS OF OPERATION, CLEANLINESS OF INSTALLATION, CONDUCTOR CONNECTIONS, MANUFACTURER, RATINGS AND GROUNDING CONNECTIONS.
2. LIGHTING FIXTURES: OPERATION CHECK.
3. LIGHTING CONTROL SYSTEMS: ENGAGE A FACTORY-AUTHORIZED SERVICE REPRESENTATIVE TO TEST AND INSPECT COMPONENTS, ASSEMBLIES, AND EQUIPMENT INSTALLATIONS, INCLUDING CONNECTIONS. PERFORM FULL OPERATIONAL TESTS. ADJUST CONTROLS AS NEEDED.
4. TEST ALL CIRCUITS FOR PROPER FUNCTIONING AND CONNECTION.
5. EMERGENCY POWER AND LIGHTING SYSTEMS VERIFICATION OF CONNECTION TO THE BUILDING EMERGENCY POWER.
6. ELECTRICAL CURRENT READINGS IN ALL PANELBOARDS AFFECTED BY WORK TO VERIFY BALANCING OF LOADS.
7. FOR ALL LOW VOLTAGE ELECTRICAL POWER CONDUCTORS AND CABLE, PERFORM EACH VISUAL AND MECHANICAL INSPECTION AND ELECTRICAL TEST STATED IN NETA ACCEPTANCE TESTING SPECIFICATION. CERTIFY COMPLIANCE WITH TEST PARAMETERS.
8. FOR PANELBOARDS, PERFORM EACH VISUAL AND MECHANICAL INSPECTION AND ELECTRICAL TEST STATED IN NETA ACCEPTANCE TESTING SPECIFICATION. CERTIFY COMPLIANCE WITH TEST PARAMETERS.
9. FOR ENCLOSED SWITCHES AND CIRCUIT BREAKERS, PERFORM EACH VISUAL AND MECHANICAL INSPECTION AND ELECTRICAL TEST STATED IN NETA ACCEPTANCE TESTING SPECIFICATION. CERTIFY COMPLIANCE WITH TEST PARAMETERS.

E-12 TEMPORARY LIGHT AND POWER

A. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND UPON COMPLETION, REMOVE SYSTEM OF TEMPORARY LIGHTING AND POWER FOR THE USE OF ALL CONSTRUCTION TRADES AS NECESSARY.

B. WIRING SHALL BE PROVIDED FOR TEMPORARY USE DURING BUILDING CONSTRUCTION, INCLUDING GROUNDING AND FUSED MAIN CUT-OFF SWITCHES. TEMPORARY ELECTRIC LINES WITH BRANCH SWITCHES SHALL BE PROVIDED FOR LIGHTING AND FOR TAPS FOR ELECTRIC TOOLS, PUMPS AND OTHER TEMPORARY EQUIPMENT. ALL EQUIVALENT TO A MAIN LINE LOOPED THROUGH FLOOR SPACES AND UP STAIRWELLS OR SHAFTS. ALL POWER OUTLETS SHALL BE GROUNDED TO AN EQUIPMENT GROUND WIRE IN AN APPROVED MANNER. ELECTRIC LINES SHALL BE EXTENDED TO POWER TOOLS WHICH CANNOT BE LOCATED WITHIN REACH OF EXTENSION CORDS.

C. LIGHT BULBS SHALL BE PROVIDED IN SUFFICIENT QUANTITY TO LIGHT THE BUILDING FOR SAFETY PURPOSES. EXTENSION CORDS SHALL BE PROVIDED AS MAY BE ESSENTIAL TO THE PROPER EXECUTION OF THE WORK.

D. TEMPORARY LIGHTING SHALL BE PROVIDED FOR ALL STAIRS AND OTHER LOCATIONS WHERE NEEDED FOR SAFETY OR THE PROPER EXECUTION OF WORK AND SHALL CONFORM TO ALL OSHA STANDARDS.

E. THE ELECTRICAL CONTRACTOR SHALL MAINTAIN TEMPORARY LIGHTING AND POWER SYSTEMS IN GOOD WORKING CONDITION, INCLUDING THE RELOCATION AND REINSTALLATION WHEN REQUIRED TO AVOID INTERFERENCE WITH THE PROGRESS OF CONSTRUCTION.

F. PROVIDE GROUND FAULT PERSONNEL PROTECTION FOR ALL SINGLE PHASE, 15 AND 20 AMPERE RECEPTACLES. ALL RECEPTACLES AND PORTABLE CORD CONNECTORS SHALL HAVE NEMA STANDARD LOCKING TYPE CONFIGURATIONS AND SHALL CONFORM TO ALL OSHA STANDARDS.

Table with 3 columns: NO., REVISION, DATE. Rows include SITE PLAN APPLICATION, REVISIONS PER TOWN COMMENTS, ISSUED FOR REVIEW, REVISED PER TOWN AND OGS COMMENTS.

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TITLE:

ELECTRICAL SPECS. CONT.

Scale: Date: 04/24/2023 Drawn By: JV Checked By: JL Project No.: NDT50004.00 Sheet No.: 3 of 7 Dwg. No.: E003



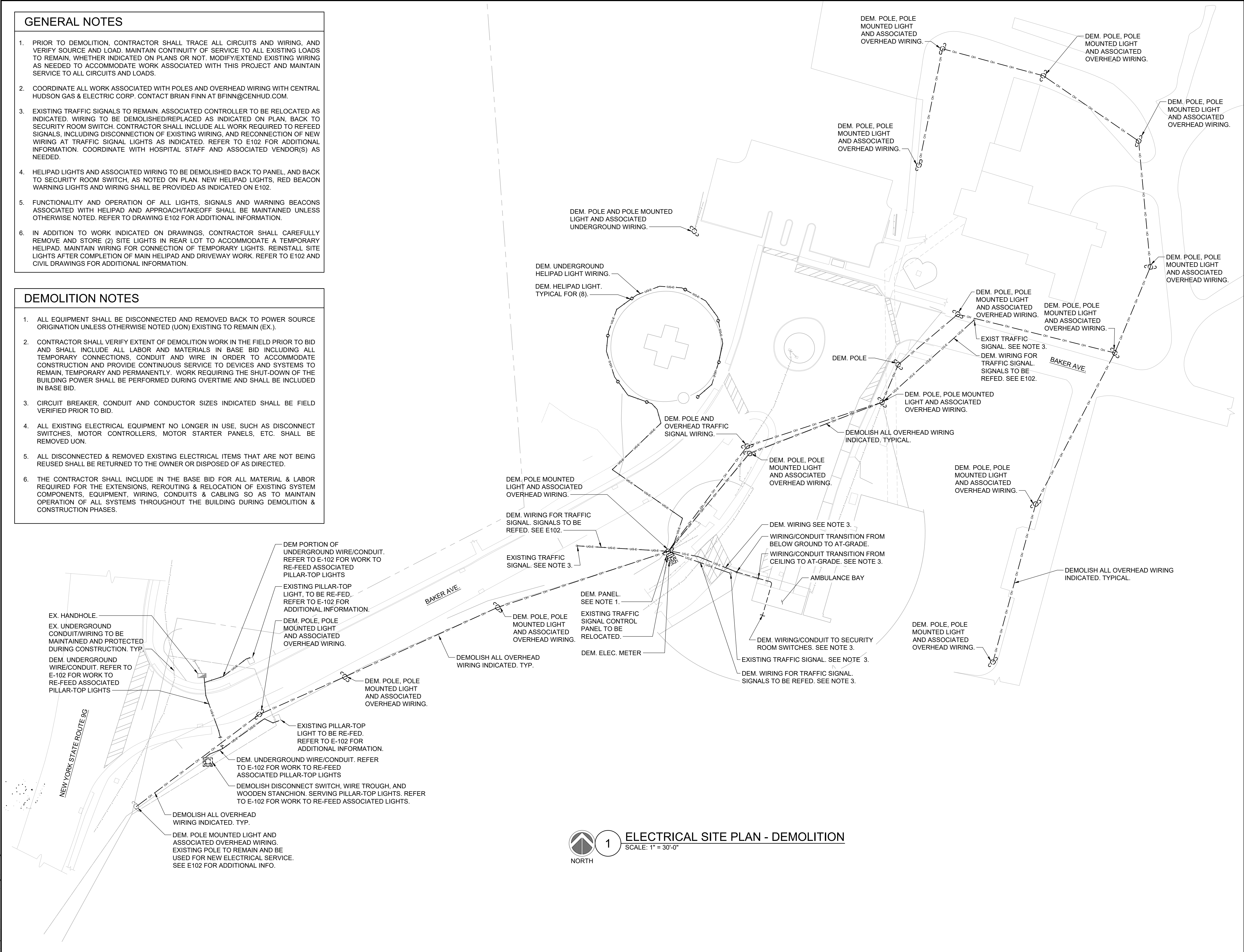
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**GENERAL NOTES**

- PRIOR TO DEMOLITION, CONTRACTOR SHALL TRACE ALL CIRCUITS AND WIRING, AND VERIFY SOURCE AND LOAD. MAINTAIN CONTINUITY OF SERVICE TO ALL EXISTING LOADS TO REMAIN, WHETHER INDICATED ON PLANS OR NOT. MODIFY/EXTEND EXISTING WIRING AS NEEDED TO ACCOMMODATE WORK ASSOCIATED WITH THIS PROJECT AND MAINTAIN SERVICE TO ALL CIRCUITS AND LOADS.
- COORDINATE ALL WORK ASSOCIATED WITH POLES AND OVERHEAD WIRING WITH CENTRAL HUDSON GAS & ELECTRIC CORP. CONTACT BRIAN FINN AT BFINN@CENHUD.COM.
- EXISTING TRAFFIC SIGNALS TO REMAIN. ASSOCIATED CONTROLLER TO BE RELOCATED AS INDICATED. WIRING TO BE DEMOLISHED/REPLACED AS INDICATED ON PLAN. BACK TO SECURITY ROOM SWITCH. CONTRACTOR SHALL INCLUDE ALL WORK REQUIRED TO REFEED SIGNALS, INCLUDING DISCONNECTION OF EXISTING WIRING, AND RECONNECTION OF NEW WIRING AT TRAFFIC SIGNAL LIGHTS AS INDICATED. REFER TO E102 FOR ADDITIONAL INFORMATION. COORDINATE WITH HOSPITAL STAFF AND ASSOCIATED VENDOR(S) AS NEEDED.
- HELIPAD LIGHTS AND ASSOCIATED WIRING TO BE DEMOLISHED BACK TO PANEL, AND BACK TO SECURITY ROOM SWITCH, AS NOTED ON PLAN. NEW HELIPAD LIGHTS, RED BEACON WARNING LIGHTS AND WIRING SHALL BE PROVIDED AS INDICATED ON E102.
- FUNCTIONALITY AND OPERATION OF ALL LIGHTS, SIGNALS AND WARNING BEACONS ASSOCIATED WITH HELIPAD AND APPROACH/TAKEOFF SHALL BE MAINTAINED UNLESS OTHERWISE NOTED. REFER TO DRAWING E102 FOR ADDITIONAL INFORMATION.
- IN ADDITION TO WORK INDICATED ON DRAWINGS, CONTRACTOR SHALL CAREFULLY REMOVE AND STORE (2) SITE LIGHTS IN REAR LOT TO ACCOMMODATE A TEMPORARY HELIPAD. MAINTAIN WIRING FOR CONNECTION OF TEMPORARY LIGHTS. REINSTALL SITE LIGHTS AFTER COMPLETION OF MAIN HELIPAD AND DRIVEWAY WORK. REFER TO E102 AND CIVIL DRAWINGS FOR ADDITIONAL INFORMATION.

**DEMOLITION NOTES**

- ALL EQUIPMENT SHALL BE DISCONNECTED AND REMOVED BACK TO POWER SOURCE ORIGIN UNLESS OTHERWISE NOTED (UON) EXISTING TO REMAIN (EX.).
- CONTRACTOR SHALL VERIFY EXTENT OF DEMOLITION WORK IN THE FIELD PRIOR TO BID AND SHALL INCLUDE ALL LABOR AND MATERIALS IN BASE BID INCLUDING ALL TEMPORARY CONNECTIONS, CONDUIT AND WIRE IN ORDER TO ACCOMMODATE CONSTRUCTION AND PROVIDE CONTINUOUS SERVICE TO DEVICES AND SYSTEMS TO REMAIN, TEMPORARY AND PERMANENTLY. WORK REQUIRING THE SHUT-DOWN OF THE BUILDING POWER SHALL BE PERFORMED DURING OVERTIME AND SHALL BE INCLUDED IN BASE BID.
- CIRCUIT BREAKER, CONDUIT AND CONDUCTOR SIZES INDICATED SHALL BE FIELD VERIFIED PRIOR TO BID.
- ALL EXISTING ELECTRICAL EQUIPMENT NO LONGER IN USE, SUCH AS DISCONNECT SWITCHES, MOTOR CONTROLLERS, MOTOR STARTER PANELS, ETC. SHALL BE REMOVED UON.
- ALL DISCONNECTED & REMOVED EXISTING ELECTRICAL ITEMS THAT ARE NOT BEING REUSED SHALL BE RETURNED TO THE OWNER OR DISPOSED OF AS DIRECTED.
- THE CONTRACTOR SHALL INCLUDE IN THE BASE BID FOR ALL MATERIAL & LABOR REQUIRED FOR THE EXTENSIONS, REROUTING & RELOCATION OF EXISTING SYSTEM COMPONENTS, EQUIPMENT, WIRING, CONDUITS & CABLING SO AS TO MAINTAIN OPERATION OF ALL SYSTEMS THROUGHOUT THE BUILDING DURING DEMOLITION & CONSTRUCTION PHASES.



**1 ELECTRICAL SITE PLAN - DEMOLITION**  
SCALE: 1" = 30'-0"  
NORTH

| NO. | REVISION                          | DATE       |
|-----|-----------------------------------|------------|
| 1   | SITE PLAN APPLICATION             | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS       | 06/22/2023 |
| 3   | ISSUED FOR REVIEW                 | 08/18/2023 |
| 4   | REVISED PER TOWN AND OGS COMMENTS | 08/23/2023 |
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TITLE:  
**ELECTRICAL SITE PLAN - DEMOLITION**

Scale: 1" = 30'  
Date: 04/24/2023  
Drawn By: JV  
Checked By: JL  
Project No.: NDT50004.00  
Sheet No.: 4 of 7  
Dwg. No.: **E101**

STATE OF NEW YORK  
JOHN TORE  
LICENSED PROFESSIONAL ENGINEER  
071678  
EXPIRES 08/30/2024

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**'SP' PANEL SCHEDULE (REPLACEMENT PANEL)**

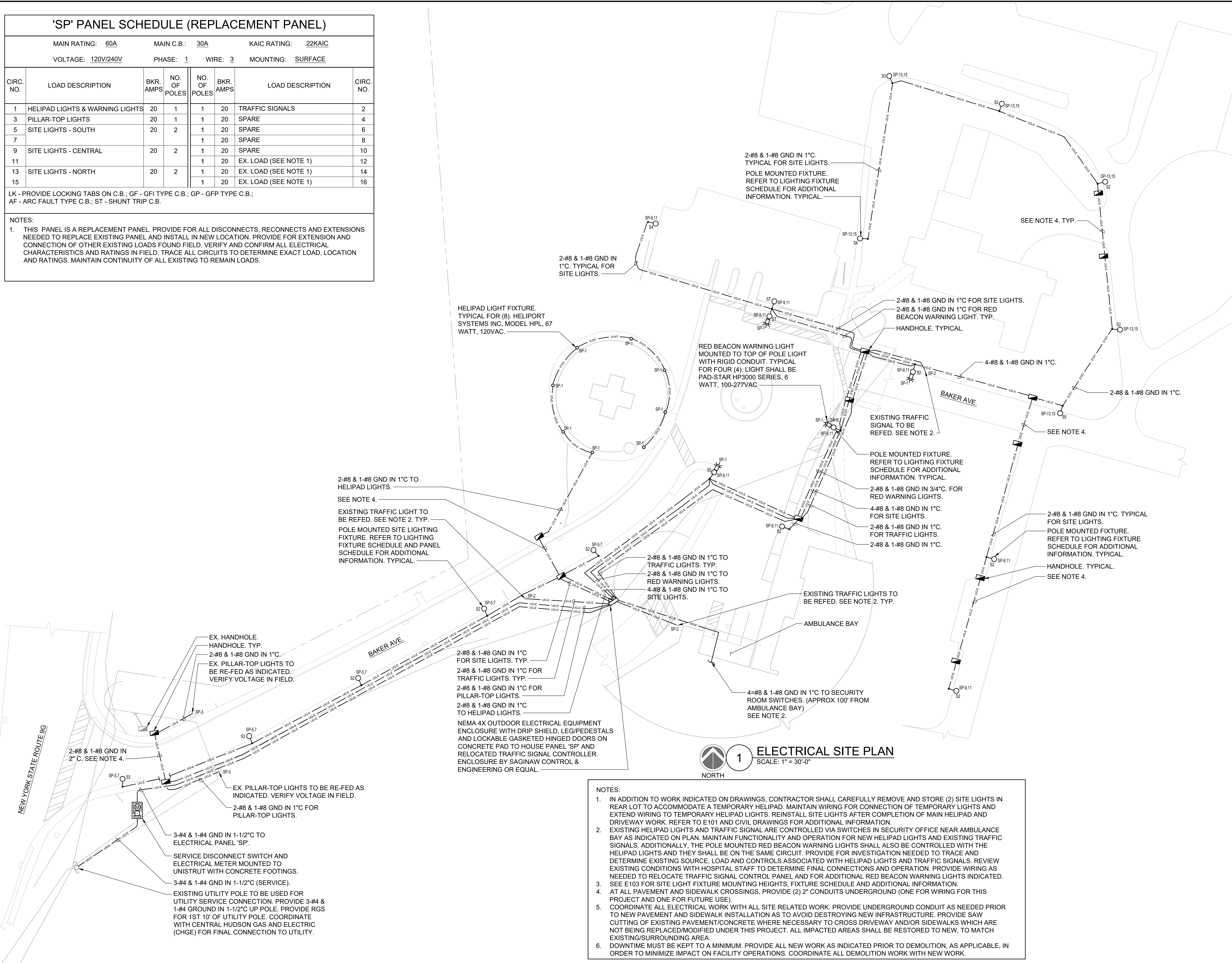
MAIN RATING: **60A** MAIN C.B.: **30A** KAIC RATING: **22KAIC**  
 VOLTAGE: **120V/240V** PHASE: **1** WIRE: **3** MOUNTING: **SURFACE**

| CIRC. NO. | LOAD DESCRIPTION                | BKR. AMPS | NO. OF POLES | NO. OF POLES |              | LOAD DESCRIPTION      | CIRC. NO. |
|-----------|---------------------------------|-----------|--------------|--------------|--------------|-----------------------|-----------|
|           |                                 |           |              | BKR. AMPS    | NO. OF POLES |                       |           |
| 1         | HELIPAD LIGHTS & WARNING LIGHTS | 20        | 1            | 1            | 20           | TRAFFIC SIGNALS       | 2         |
| 3         | PILLAR-TOP LIGHTS               | 20        | 1            | 1            | 20           | SPARE                 | 4         |
| 5         | SITE LIGHTS - SOUTH             | 20        | 2            | 1            | 20           | SPARE                 | 6         |
| 7         |                                 |           |              | 1            | 20           | SPARE                 | 8         |
| 9         | SITE LIGHTS - CENTRAL           | 20        | 2            | 1            | 20           | SPARE                 | 10        |
| 11        |                                 |           |              | 1            | 20           | EX. LOAD (SEE NOTE 1) | 12        |
| 13        | SITE LIGHTS - NORTH             | 20        | 2            | 1            | 20           | EX. LOAD (SEE NOTE 1) | 14        |
| 15        |                                 |           |              | 1            | 20           | EX. LOAD (SEE NOTE 1) | 16        |

LK - PROVIDE LOCKING TABS ON C.B.; GF - GFI TYPE C.B.; GP - GFP TYPE C.B.;  
 AF - ARC FAULT TYPE C.B.; ST - SHUNT TRIP C.B.

NOTES:  
 1. THIS PANEL IS A REPLACEMENT PANEL. PROVIDE FOR ALL DISCONNECTS, RECONNECTS AND EXTENSIONS NEEDED TO REPLACE EXISTING PANEL AND INSTALL IN NEW LOCATION. PROVIDE FOR EXTENSION AND CONNECTION OF OTHER EXISTING LOADS FOUND FIELD. VERIFY AND CONFIRM ALL ELECTRICAL CHARACTERISTICS AND RATINGS IN FIELD. TRACE ALL CIRCUITS TO DETERMINE EXACT LOAD, LOCATION AND RATINGS. MAINTAIN CONTINUITY OF ALL EXISTING TO REMAIN LOADS.

| NO. | REVISION                          | DATE       |
|-----|-----------------------------------|------------|
| 1   | SITE PLAN APPLICATION             | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS       | 06/22/2023 |
| 3   | ISSUED FOR REVIEW                 | 08/18/2023 |
| 4   | REVISED PER TOWN AND OGS COMMENTS | 08/23/2023 |



**1 ELECTRICAL SITE PLAN**  
 SCALE: 1" = 30'-0"  
 NORTH

- NOTES:
- IN ADDITION TO WORK INDICATED ON DRAWINGS, CONTRACTOR SHALL CAREFULLY REMOVE AND STORE (2) SITE LIGHTS IN REAR LOT TO ACCOMMODATE A TEMPORARY HELIPAD. MAINTAIN WIRING FOR CONNECTION OF TEMPORARY LIGHTS AND EXTEND WIRING TO TEMPORARY HELIPAD LIGHTS. REINSTALL SITE LIGHTS AFTER COMPLETION OF MAIN HELIPAD AND DRIVEWAY WORK. REFER TO E101 AND CIVIL DRAWINGS FOR ADDITIONAL INFORMATION.
  - EXISTING HELIPAD LIGHTS AND TRAFFIC SIGNAL ARE CONTROLLED VIA SWITCHES IN SECURITY OFFICE NEAR AMBULANCE BAY AS INDICATED ON PLAN. MAINTAIN FUNCTIONALITY AND OPERATION FOR NEW HELIPAD LIGHTS AND EXISTING TRAFFIC SIGNALS. ADDITIONALLY, THE POLE MOUNTED RED BEACON WARNING LIGHTS SHALL ALSO BE CONTROLLED WITH THE HELIPAD LIGHTS AND THEY SHALL BE ON THE SAME CIRCUIT. PROVIDE FOR INVESTIGATION NEEDED TO TRACE AND DETERMINE EXISTING SOURCE, LOAD AND CONTROLS ASSOCIATED WITH HELIPAD LIGHTS AND TRAFFIC SIGNALS. REVIEW EXISTING CONDITIONS WITH HOSPITAL STAFF TO DETERMINE FINAL CONNECTIONS AND OPERATION. PROVIDE WIRING AS NEEDED TO RELOCATE TRAFFIC SIGNAL CONTROL PANEL AND FOR ADDITIONAL RED BEACON WARNING LIGHTS INDICATED.
  - SEE E103 FOR SITE LIGHT FIXTURE MOUNTING HEIGHTS, FIXTURE SCHEDULE AND ADDITIONAL INFORMATION.
  - AT ALL PAVEMENT AND SIDEWALK CROSSINGS, PROVIDE (2) 2" CONDUITS UNDERGROUND (ONE FOR WIRING FOR THIS PROJECT AND ONE FOR FUTURE USE).
  - COORDINATE ALL ELECTRICAL WORK WITH ALL SITE RELATED WORK. PROVIDE UNDERGROUND CONDUIT AS NEEDED PRIOR TO NEW PAVEMENT AND SIDEWALK INSTALLATION AS TO AVOID DESTROYING NEW INFRASTRUCTURE. PROVIDE SAW CUTTING OF EXISTING PAVEMENT/CONCRETE WHERE NECESSARY TO CROSS DRIVEWAY AND/OR SIDEWALKS WHICH ARE NOT BEING REPLACED/MODIFIED UNDER THIS PROJECT. ALL IMPACTED AREAS SHALL BE RESTORED TO NEW, TO MATCH EXISTING/SURROUNDING AREA.
  - DOWNTIME MUST BE KEPT TO A MINIMUM. PROVIDE ALL NEW WORK AS INDICATED PRIOR TO DEMOLITION, AS APPLICABLE, IN ORDER TO MINIMIZE IMPACT ON FACILITY OPERATIONS. COORDINATE ALL DEMOLITION WORK WITH NEW WORK.

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TITLE: **ELECTRICAL SITE PLAN**

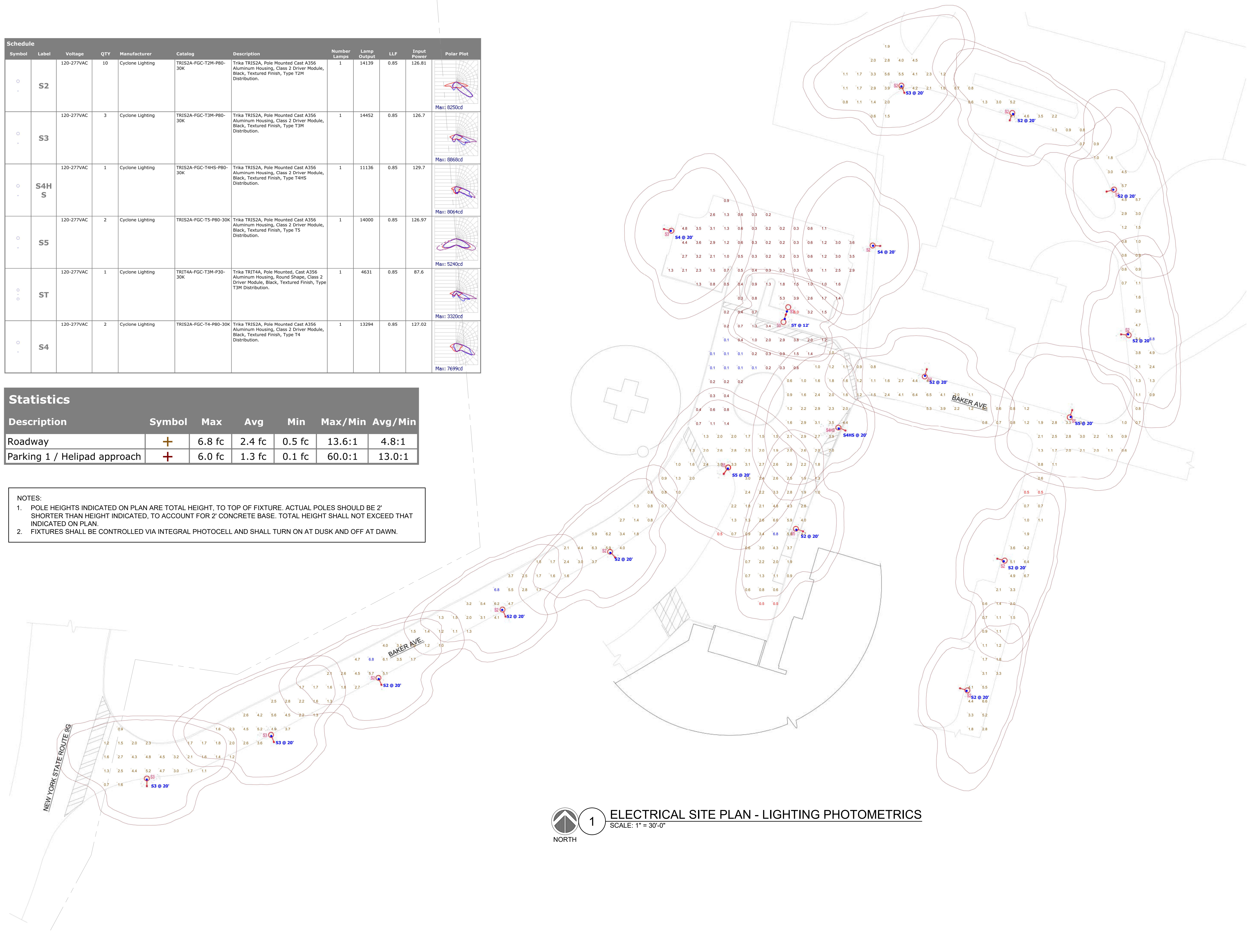
Scale: 1" = 30'  
 Date: 04/24/2023  
 Drawn By: JV  
 Checked By: JL  
 Project No.: NDT50004.00  
 Sheet No.: 5 of 7  
 Dwg. No.: **E102**

| NO. | REVISION                          | DATE       |
|-----|-----------------------------------|------------|
| 1   | SITE PLAN APPLICATION             | 04/24/2023 |
| 2   | REVISIONS PER TOWN COMMENTS       | 06/22/2023 |
| 3   | ISSUED FOR REVIEW                 | 08/18/2023 |
| 4   | REVISED PER TOWN AND OGS COMMENTS | 08/23/2023 |

| Symbol | Label    | Voltage    | QTY | Manufacturer     | Catalog                 | Description                                                                                                                                | Number Lamps | Lamp Output | LLF  | Input Power | Polar Plot |
|--------|----------|------------|-----|------------------|-------------------------|--------------------------------------------------------------------------------------------------------------------------------------------|--------------|-------------|------|-------------|------------|
| ○      | S2       | 120-277VAC | 10  | Cyclone Lighting | TRIS2A-FGC-T2M-P80-30K  | Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T2M Distribution.               | 1            | 14139       | 0.85 | 126.81      |            |
| ○      | S3       | 120-277VAC | 3   | Cyclone Lighting | TRIS2A-FGC-T3M-P80-30K  | Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T3M Distribution.               | 1            | 14452       | 0.85 | 126.7       |            |
| ○      | S4H<br>S | 120-277VAC | 1   | Cyclone Lighting | TRIS2A-FGC-T4HS-P80-30K | Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T4HS Distribution.              | 1            | 11136       | 0.85 | 129.7       |            |
| ○      | S5       | 120-277VAC | 2   | Cyclone Lighting | TRIS2A-FGC-T5-P80-30K   | Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T5 Distribution.                | 1            | 14000       | 0.85 | 126.97      |            |
| ○      | ST       | 120-277VAC | 1   | Cyclone Lighting | TRIT4A-FGC-T3M-P30-30K  | Trika TRIT4A, Pole Mounted, Cast A356 Aluminum Housing, Round Shape, Class 2 Driver Module, Black, Textured Finish, Type T3M Distribution. | 1            | 4631        | 0.85 | 87.6        |            |
| ○      | S4       | 120-277VAC | 2   | Cyclone Lighting | TRIS2A-FGC-T4-P80-30K   | Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T4 Distribution.                | 1            | 13294       | 0.85 | 127.02      |            |

| Statistics                   |        |        |        |        |         |         |
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| Description                  | Symbol | Max    | Avg    | Min    | Max/Min | Avg/Min |
| Roadway                      | +      | 6.8 fc | 2.4 fc | 0.5 fc | 13.6:1  | 4.8:1   |
| Parking 1 / Helipad approach | +      | 6.0 fc | 1.3 fc | 0.1 fc | 60.0:1  | 13.0:1  |

NOTES:  
1. POLE HEIGHTS INDICATED ON PLAN ARE TOTAL HEIGHT, TO TOP OF FIXTURE. ACTUAL POLES SHOULD BE 2' SHORTER THAN HEIGHT INDICATED, TO ACCOUNT FOR 2' CONCRETE BASE. TOTAL HEIGHT SHALL NOT EXCEED THAT INDICATED ON PLAN.  
2. FIXTURES SHALL BE CONTROLLED VIA INTEGRAL PHOTOCELL AND SHALL TURN ON AT DUSK AND OFF AT DAWN.



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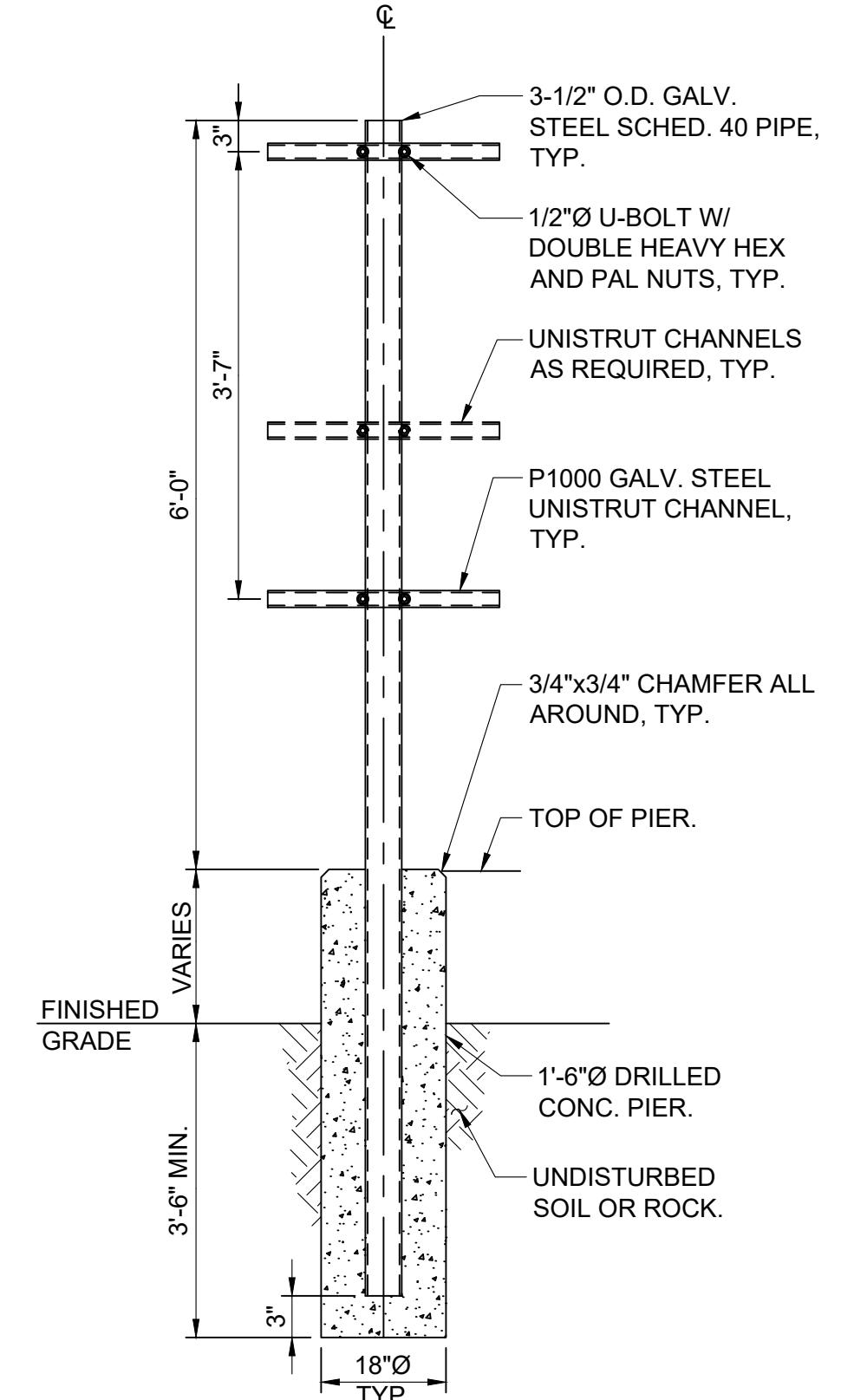
ola.com CA#: 0018400

TITLE:  
**ELECTRICAL SITE PLAN - LIGHTING PHOTOMETRICS**

Scale: 1" = 30'  
Date: 04/24/2023  
Drawn By: JV  
Checked By: JL  
Project No.: NDT50004.00  
Sheet No.: 6 of 7  
Dwg. No.: **E103**

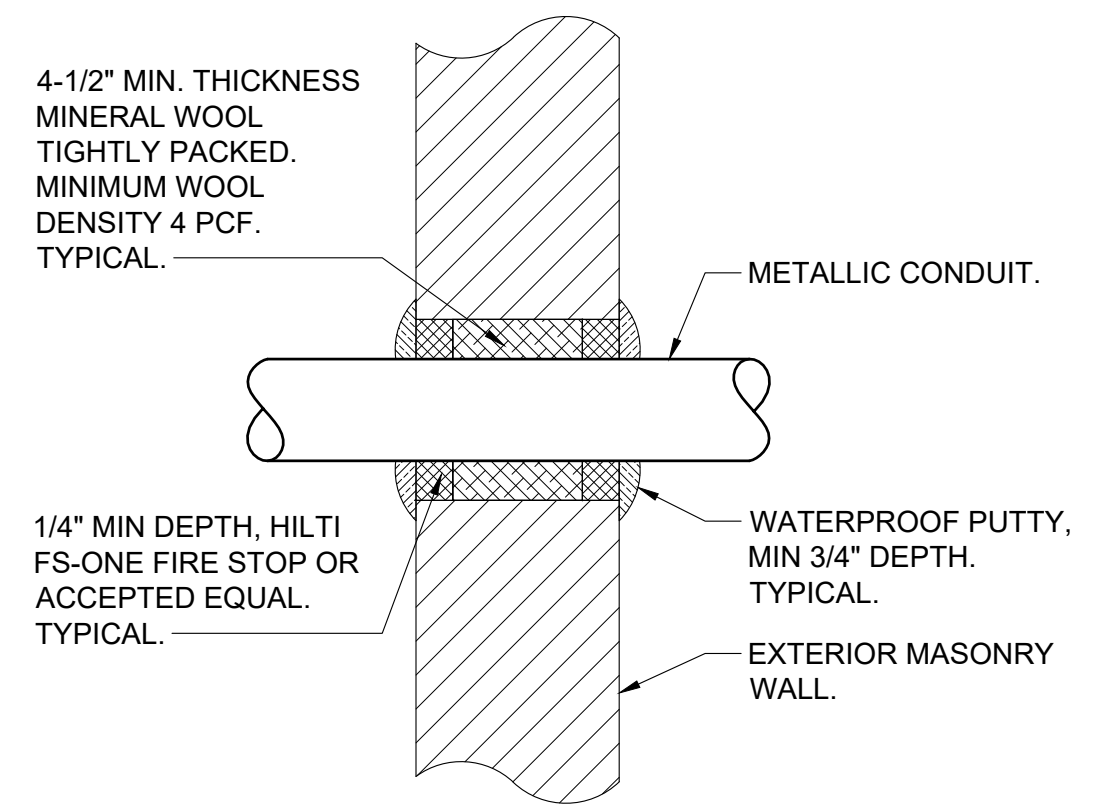
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| NO. | REVISION                          | DATE       |
|-----|-----------------------------------|------------|
| 1   | SITE PLAN APPLICATION             | 04/24/2023 |
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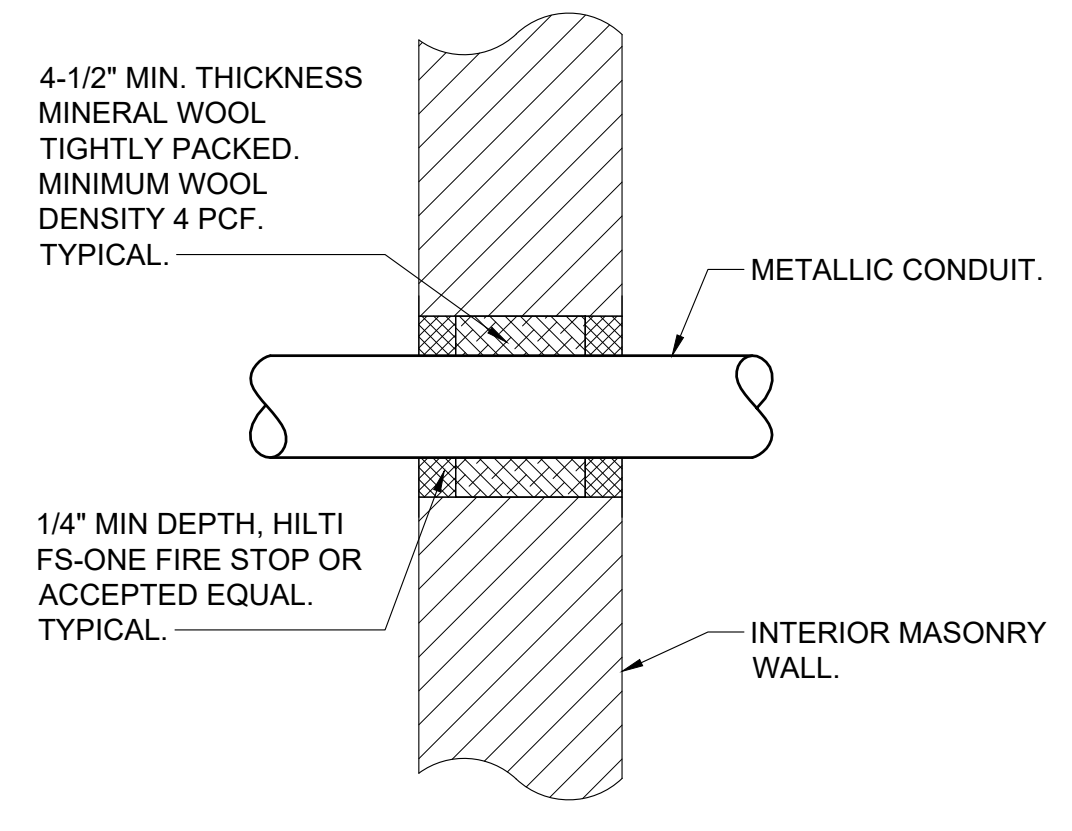


**6 EQUIPMENT SUPPORT DETAIL**  
SCALE: NONE

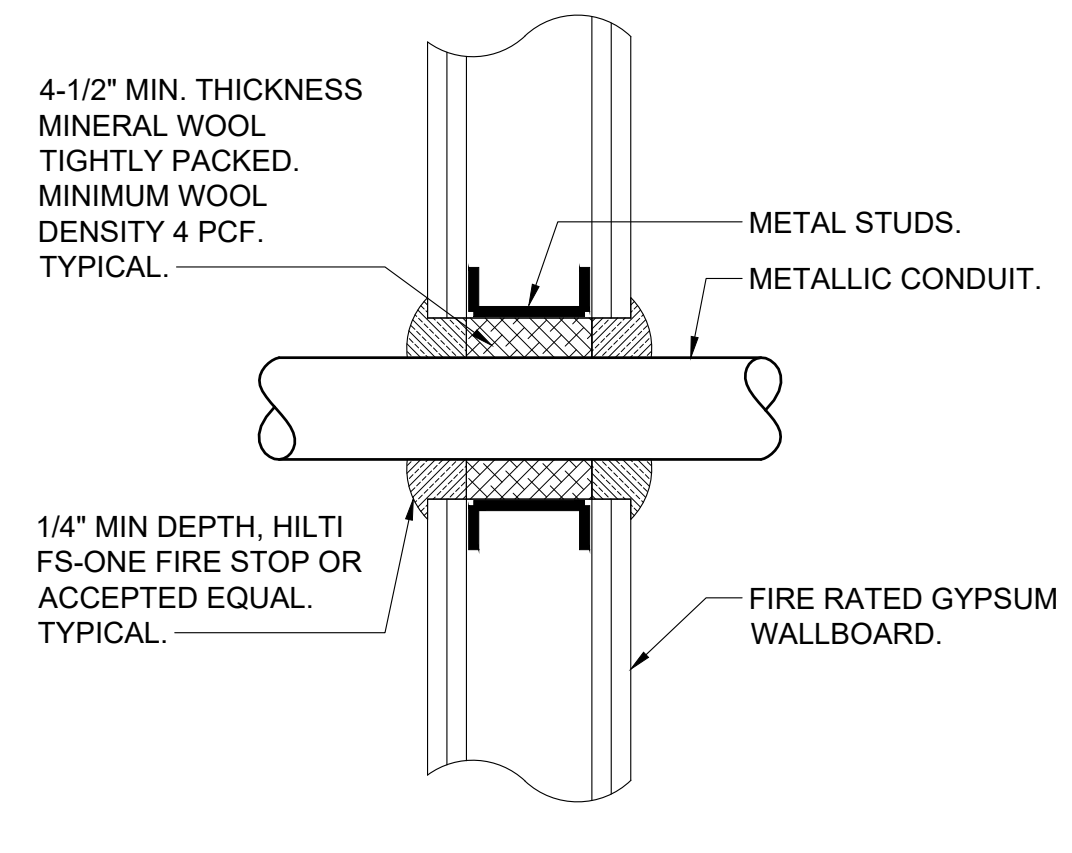
NOTES:  
1.) DESIGN TYPICAL FOR MOUNTING SINGLE PIECES OF WALL MOUNTED POWER EQUIPMENT (DISCONNECT SWITCHES, PANELBOARDS, ETC.). COORDINATE FINAL SIZE WITH EQUIPMENT.



**9 TYPICAL EXTERIOR MASONRY WALL ABOVE GRADE CONDUIT PENETRATION DETAIL**  
SCALE: NONE

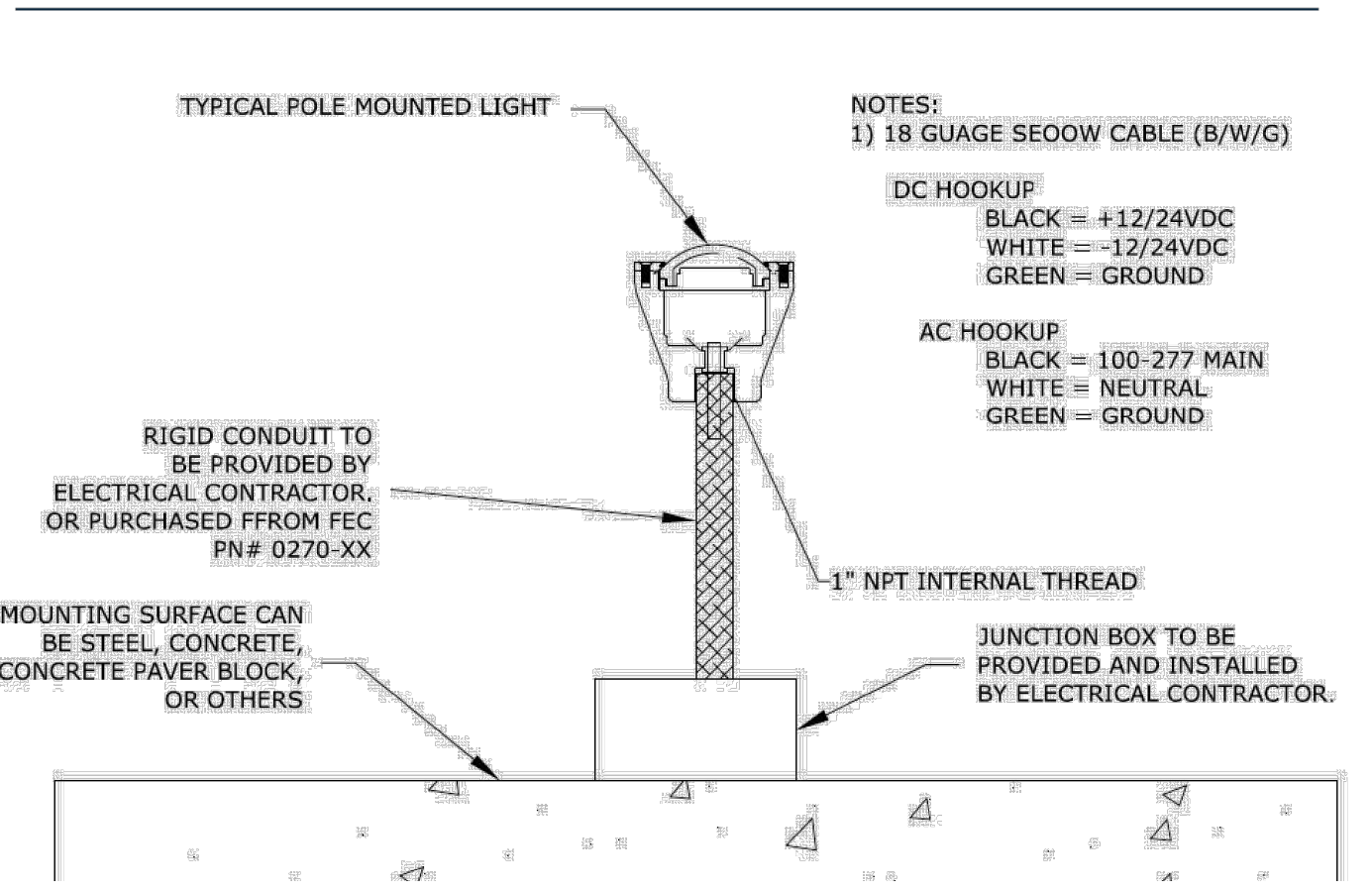


**8 TYPICAL INTERIOR MASONRY WALL CONDUIT PENETRATION DETAIL**  
SCALE: NONE

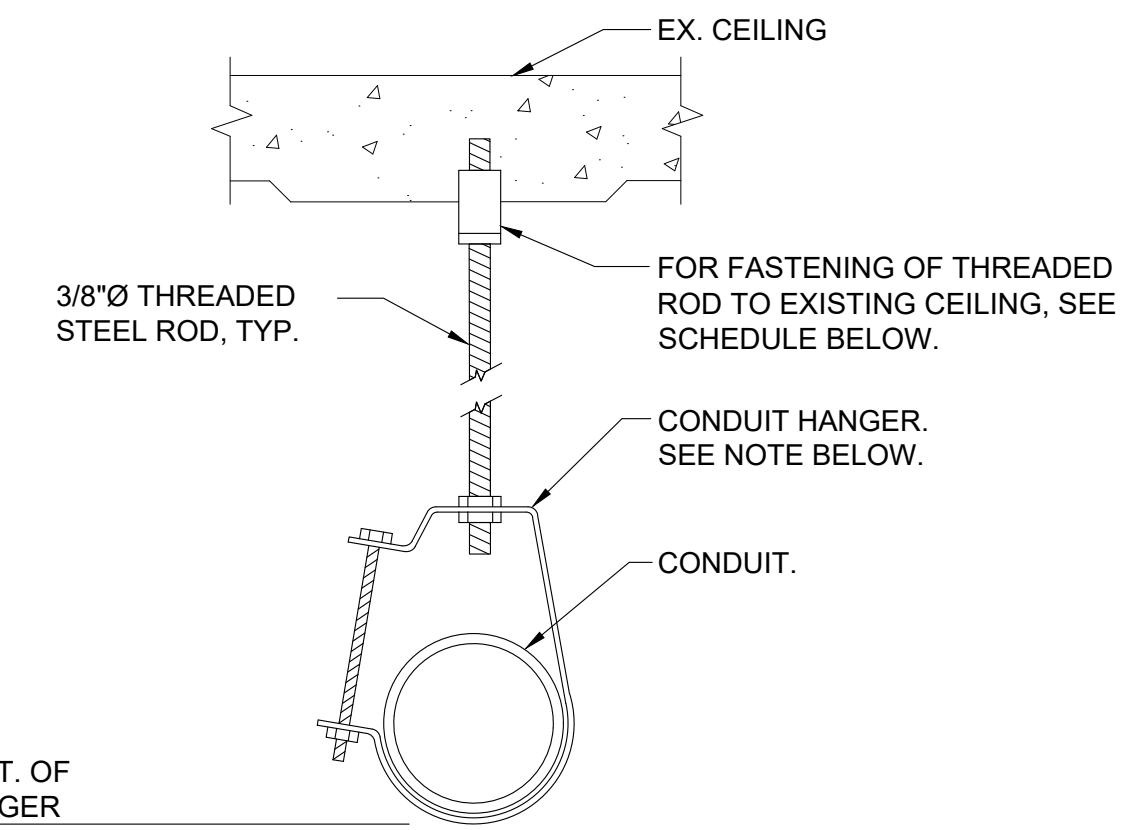


**7 TYPICAL FIRE RATED GYPSUM WALL CONDUIT PENETRATION DETAIL**  
SCALE: NONE

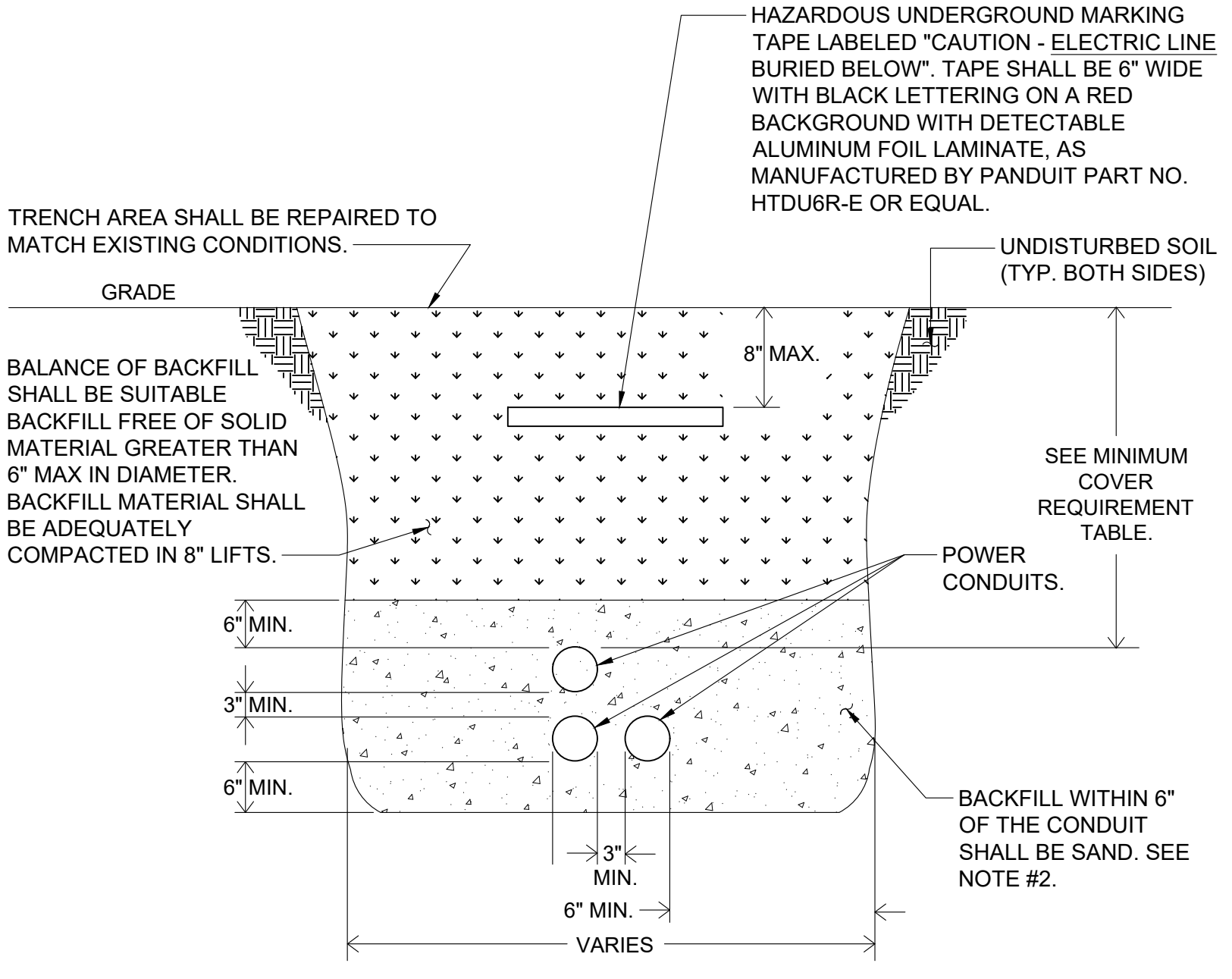
**Pole Mounting Details**



**5 TYPICAL RED BEACON WARNING LIGHT MOUNTING DETAIL**  
SCALE: NONE



**4 SINGLE CONDUIT HANGER DETAIL**  
SCALE: NONE

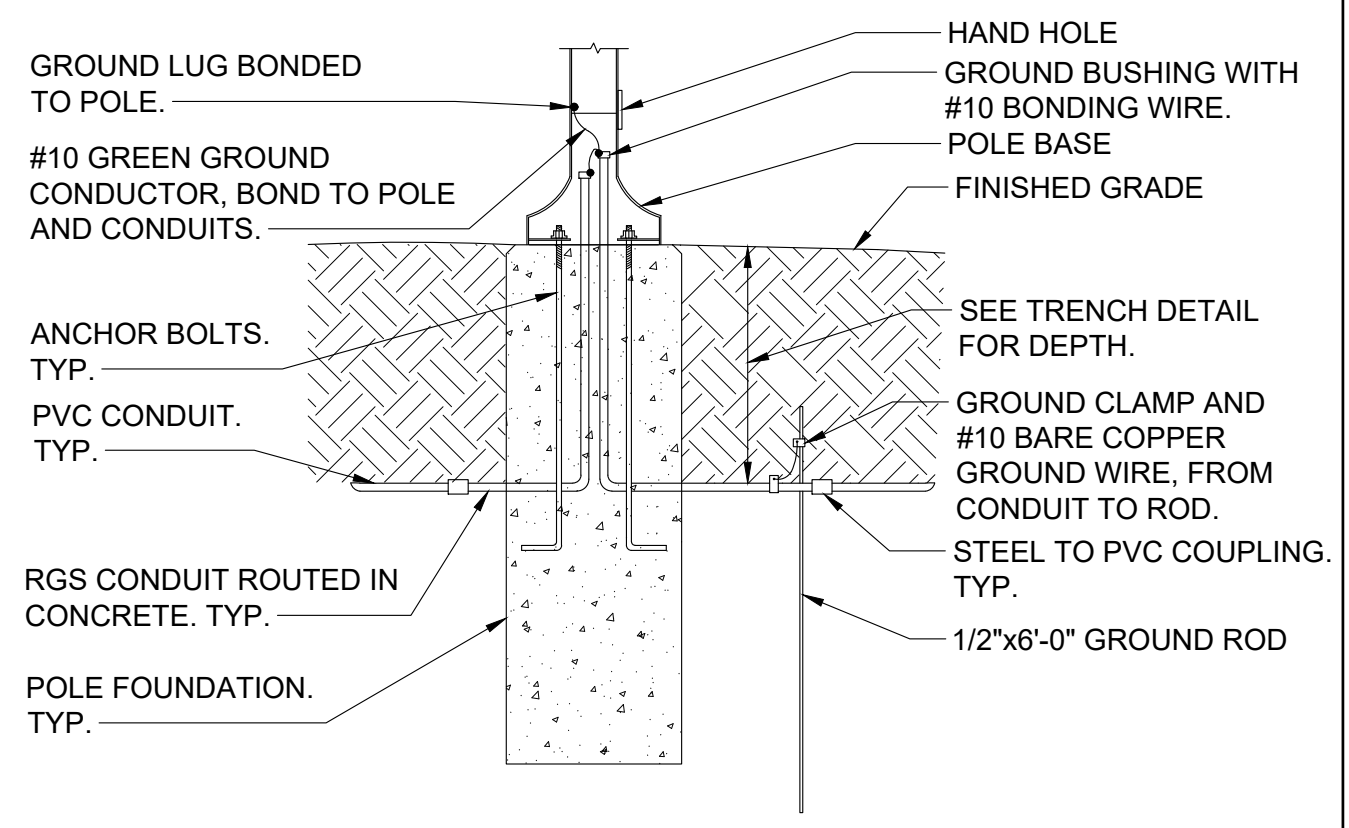


**3 TRENCHING DETAIL FOR CONDUIT**  
SCALE: NONE

**MINIMUM COVER REQUIREMENT TABLE**

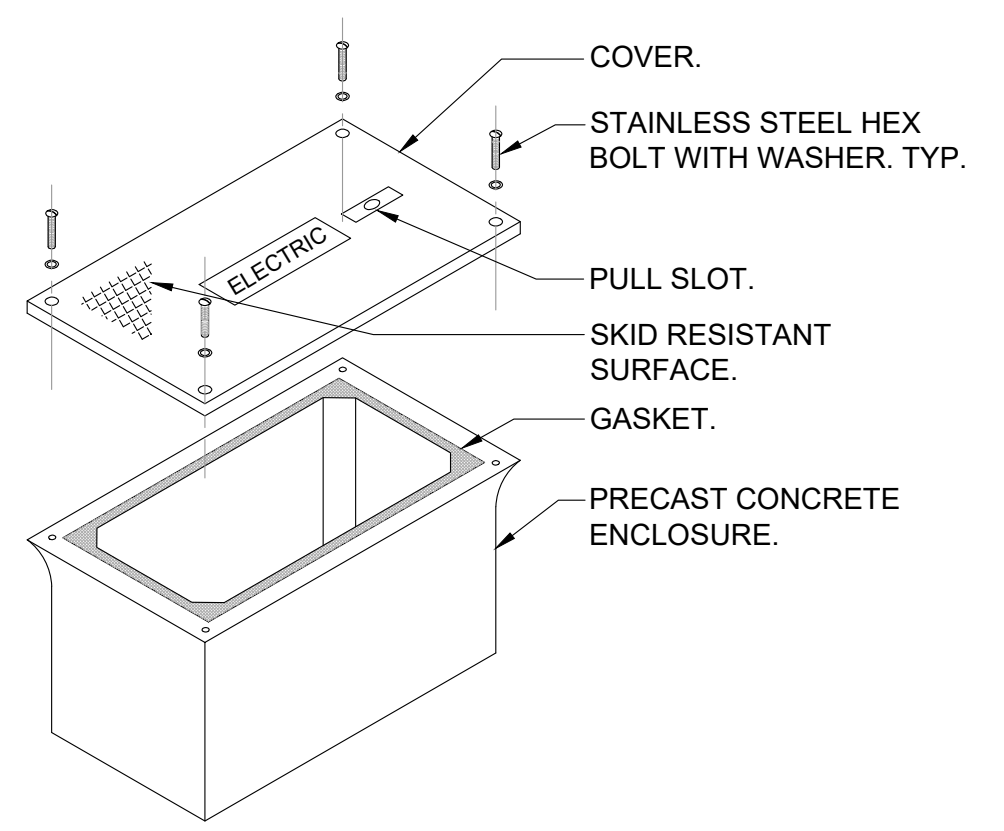
| LOCATION                                                                                                                                                      | NONMETALLIC RACEWAYS LISTED FOR DIRECT BURIAL WITHOUT CONCRETE ENCASEMENT OR OTHER APPROVED RACEWAYS | COVER           |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------|-----------------|
| ALL LOCATION NOT SPECIFIED BELOW.                                                                                                                             |                                                                                                      | 18"             |
| IN TRENCH BELOW 2-IN. THICK CONCRETE OR EQUIVALENT.                                                                                                           |                                                                                                      | 12"             |
| UNDER MINIMUM OF 4-IN. THICK CONCRETE EXTERIOR SLAB WITH NO VEHICULAR TRAFFIC AND THE SLAB EXTENDING NOT LESS THAN 6 IN. BEYOND THE UNDERGROUND INSTALLATION. |                                                                                                      | 4" SEE NOTE #2. |
| UNDER STREETS, HIGHWAYS, ROADS, ALLEYS, DRIVEWAYS, AND PARKING LOTS.                                                                                          |                                                                                                      | 24"             |

NOTES:  
1.) DETAIL SHOWN FOR INFORMATION PURPOSES. SAME CONCEPT SHALL ALSO APPLY FOR SINGLE CONDUITS.  
2.) SAND MAY BE OMITTED FOR INSTALLATIONS WHERE COVER REQUIREMENTS ARE 6" OR LESS.



NOTE:  
1. THIS DETAIL PROVIDES ELECTRICAL GROUNDING/ BONDING AND RACEWAY INFORMATION ONLY.

**2 LIGHT POST ELECTRICAL INSTALLATION DETAIL**  
SCALE: NONE



NOTES:  
1. HAND HOLE SHALL BE 18" x 24" x 12" (UNLESS OTHERWISE NOTED ON DRAWINGS) WITH OPEN BOTTOM AND RATED FOR VEHICULAR TRAFFIC AS MANUFACTURED BY QUIAZITE, MODEL #G2436BA30 OR EQUAL. BOX SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.  
2. PROVIDE 6" OF GRAVEL BELOW BOX.  
3. COVER SHALL BE IMPRINTED WITH THE APPROPRIATE DESCRIPTION OF BOX CONTENTS (I.E. ELECTRIC, TELEPHONE, DATA, CABLE TV, ETC.).

**1 HAND HOLE DETAIL**  
SCALE: NONE

**MID-HUDSON REGIONAL HOSPITAL**  
POUGHKEEPSIE, NEW YORK



**WESTCHESTER COUNTY HEALTH CARE CORPORATION**  
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TITLE: **ELECTRICAL DETAILS**

Scale: \_\_\_\_\_  
Date: 04/24/2023  
Drawn By: JV  
Checked By: JL  
Project No.: NDT50004.00  
Sheet No.: 7 of 7  
Dwg. No.: **E701**

**GENERAL NOTES:**

- THESE SHEETS ARE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA) AND THE REQUIREMENTS OF THE 2010 PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY.
- THE DIMENSIONS SHOWN IN THE DETAILS AS MINIMUMS AND MAXIMUMS ARE THE LIMITS FOR DESIGN AND FIELD LAYOUT. FOR WORK ACCEPTANCE VALUES SEE "CRITICAL ELEMENTS FOR THE DESIGN, LAYOUT, AND ACCEPTANCE OF PEDESTRIAN FACILITIES" ON SHEET 11 OF 12 AND SHEET 12 OF 12.
- A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFYING ALL ELEVATIONS AND DIMENSIONS TO ENSURE THAT THE FINAL LAYOUT OF PEDESTRIAN FACILITIES MEETS ADA REQUIREMENTS. ANY SURVEY WORK NECESSARY TO MEET THESE REQUIREMENTS SHALL BE PAID FOR UNDER ITEM 43-00 SURVEY OPERATIONS. A CONTRACT CONTROL PLAN IS NOT NECESSARY FOR WORK LIMITED TO SIDEWALKS AND CURB RAMPS.
- WHERE PEDESTRIAN STREET CROSSINGS ARE PROVIDED AT INTERSECTIONS WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE, AND AT MIDBLOCK CROSSINGS, THE CROSS SLOPE OF THE CURB RAMP SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
- WHERE THE EXISTING ROADWAY GRADE EXCEEDS THE MAXIMUM ALLOWABLE CROSS SLOPE FOR A CURB RAMP, AND CANNOT BE CORRECTED WITHIN THE SCOPE OF THE PROJECT, THE RAMP SHOULD BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE "CURB RAMP CROSS SLOPE TRANSITION" DETAILS ON SHEET 8 OF 12. THE RAMP MAY NEED TO BE JUSTIFIED AS A NONSTANDARD FEATURE. SEE NOTE 7 ON THIS SHEET.
- RAMP SIDE OPTIONS ARE DETAILED ON SHEET 3 OF 12, WHERE A PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP. FLARED SIDES SHALL BE INSTALLED WITH A MAXIMUM SLOPE OF 9.5% FOR DESIGN AND LAYOUT, AND 10% MAXIMUM FOR WORK ACCEPTANCE. A PEDESTRIAN CIRCULATION PATH IS ASSUMED TO CROSS THE CURB RAMP WHEN AREA ADJACENT TO THE RAMP IS PAVED AND FREE OF VERTICAL OBSTRUCTIONS THAT WOULD PREVENT PEDESTRIAN PASSAGE. THERE IS NO MAXIMUM FLARE SLOPE FOR A RAMP THAT IS NOT CROSSED BY A PEDESTRIAN CIRCULATION PATH.
- THE BACK SIDE OF A PARALLEL RAMP SHOULD BE GRADED TO A MAXIMUM SLOPE OF 25% TO MATCH EXISTING TERRAIN UNLESS OTHERWISE SHOWN IN THE CONTRACT DOCUMENTS. WHERE GRADING IS NOT FEASIBLE DUE TO LIMITED ROW OR PHYSICAL CONSTRAINTS, A BACK CURB MAY BE INSTALLED. SEE DETAILS ON SHEET 3 OF 12 AND SHEET 9 OF 12.
- THE DEPARTMENT'S PREFERENCE IS TO INSTALL TWO SEPARATE CURB RAMPS AT A STREET CORNER THAT SERVES TWO SEPARATE PEDESTRIAN CROSSINGS, WITH EACH RAMP ALIGNED TO THE CROSSING THAT IT SERVES. WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT SEPARATE RAMPS, A SINGLE CURB RAMP (E.G., A DIAGONAL CURB RAMP) IS PERMITTED TO SERVE BOTH PEDESTRIAN CROSSINGS.
- TURNS AND CLEAR SPACE NOTES:**
- WHERE A CHANGE IN DIRECTION IS REQUIRED TO UTILIZE A CURB RAMP, A TURNING SPACE SHALL BE PROVIDED AT THE BASE OR THE TOP OF CURB RAMP, AS APPLICABLE. TURNING SPACES SHALL BE PERMITTED TO OVERLAP CLEAR SPACES.
- WHERE THERE ARE NO VERTICAL CONSTRAINTS AT THE BACK OF SIDEWALK, E.G., VERTICAL CURBS, BUILDINGS, FENCES) THE TURNING SPACE DIMENSIONS SHALL BE 4'-0" X 4'-0" MINIMUM WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, THE TURNING SPACE SHALL BE 4'-0" X 5'-0" MINIMUM. THE 5'-0" DIMENSION SHALL BE IN THE DIRECTION OF THE RAMP RUN.
- TURNING SPACES SHALL NOT BE DESIGNED WITH A SLOPE GREATER THAN 1.5% IN ANY DIRECTION, WHILE PROVIDING POSITIVE DRAINAGE. THE MAXIMUM SLOPE FOR WORK ACCEPTANCE IS 2.0%. THE FOLLOWING EXCEPTIONS ARE ALLOWED:
- WHERE PEDESTRIAN STREET CROSSINGS ARE PROVIDED AT INTERSECTIONS WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE, AND AT MIDBLOCK CROSSINGS, THE CROSS SLOPE OF THE TURNING SPACE SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE. WHEN A RAMP EXISTS BETWEEN THE TURNING SPACE AND THE CURB, THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.
- BELOW THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE OF 4'-0" X 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.
- BELOW THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE OF 4'-0" X 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.
- BELOW THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE OF 4'-0" X 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.
- THE MINIMUM CLEAR WIDTH FOR PEDESTRIAN ACCESS ROUTES IS 4'-0", EXCLUSIVE OF THE CURB. THE DEPARTMENT'S PREFERRED CLEAR WIDTH IS 5'-0". WHEN WALKWAY WIDTHS ARE LESS THAN 5'-0", 5'-0" X 5'-0" PASSING SPACES SHOWN IN DETAIL A OR B ON THIS SHEET OR A FEATURE OF EQUAL OR GREATER DIMENSIONS THAT MEETS THE SLOPE AND SURFACE CRITERIA, SHALL BE PROVIDED AT A MAXIMUM INTERVAL OF 200 FEET BETWEEN SIDEWALKS AND STREET CROSSINGS MAY BE PASSED SPACES, PROVIDED THEY MEET SLOPE AND SURFACE REQUIREMENTS FOR A PEDESTRIAN ACCESS ROUTE.
- THE BUFFER ZONE IS A PHYSICAL DISTANCE SEPARATING THE PEDESTRIAN ACCESS ROUTE FROM THE VEHICLE TRAVEL LANE. THE BUFFER ZONE MAY BE PAVED OR PLANTED OR A FEATURE OF EQUAL OR GREATER DIMENSIONS THAT MEETS THE SLOPE AND SURFACE CRITERIA, SHALL BE PROVIDED AT A MAXIMUM INTERVAL OF 200 FEET BETWEEN SIDEWALKS AND STREET CROSSINGS MAY BE PASSED SPACES, PROVIDED THEY MEET SLOPE AND SURFACE REQUIREMENTS FOR A PEDESTRIAN ACCESS ROUTE.
- THE MAXIMUM RECOMMENDED CROSS SLOPE OF A TYPICAL BUFFER ZONE OR SLOPE TRANSITION BEHIND SIDEWALK IS 25%. BUFFER ZONES WITH A CROSS SLOPE GREATER THAN 25% SHOULD BE PAVED, PLANTED OR CONSTRUCTED WITH HEDGING MATERIAL.
- WHEN CROSSING DRIVEWAYS, THE WORK SHALL BE IN CONFORMANCE WITH STANDARD SHEET 608-03.
- FOR PEDESTRIAN SIGNALS AND PEDESTRIAN PUSH BUTTONS, REFER TO SHEET 12 OF 12 AND STANDARD SHEET 608-10 FOR DETAILS.
- WHERE EXISTING ROADWAYS ARE SANCTUARY TO INSTALL CURBING AND/OR SIDEWALKS, THE ROADWAY SHOULD BE SANCTUARY AT LEAST 2'-0" FROM THE PROPOSED CURB LINE TO ALLOW FOR URGENT REPAIRS. THE CURB LINE, IF THE SANCTUARY IS LESS THAN 2'-0" FROM THE PROPOSED CURB LINE, THE ROADWAY SHALL BE REBUILT USING CLASS A, C, OR D CONCRETE. SEE DETAILS ON SHEET 7 OF 12.

**CURB RAMP NOTES:**

- THE MINIMUM CLEAR WIDTH OF A CURB RAMP SHALL BE 4'-0". THE DEPARTMENT'S PREFERRED CLEAR WIDTH IS 5'-0".
- THE MAXIMUM GRADING SLOPE FOR DESIGN AND LAYOUT OF A CURB RAMP SHALL BE 7.5% THE CROSS SLOPE FOR WORK ACCEPTANCE SHALL BE A MAXIMUM OF 8.5%.
- WHERE THE TERRAIN DOES NOT ALLOW CONSTRUCTION OF A CURB RAMP WITH A GRADE (RUNNING SLOPE) OF 8.5% OR LESS WITHIN 15'-0" OF THE RAMP LENGTH SHALL NOT BE REQUIRED TO EXCEED 15'-0" FOR DESIGN AND LAYOUT OR 15'-0" FOR WORK ACCEPTANCE.
- THE CROSS SLOPE OF THE CURB RAMP SHALL BE AS FLAT AS POSSIBLE AND STILL PROVIDE POSITIVE DRAINAGE. THE CROSS SLOPE OF A CURB RAMP SHALL BE 1.5% MAXIMUM FOR DESIGN AND LAYOUT, AND 2% MAXIMUM FOR WORK ACCEPTANCE.
- WHERE PEDESTRIAN STREET CROSSINGS ARE PROVIDED AT INTERSECTIONS WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE, AND AT MIDBLOCK CROSSINGS, THE CROSS SLOPE OF THE CURB RAMP SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
- WHERE THE EXISTING ROADWAY GRADE EXCEEDS THE MAXIMUM ALLOWABLE CROSS SLOPE FOR A CURB RAMP, AND CANNOT BE CORRECTED WITHIN THE SCOPE OF THE PROJECT, THE RAMP SHOULD BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE "CURB RAMP CROSS SLOPE TRANSITION" DETAILS ON SHEET 8 OF 12. THE RAMP MAY NEED TO BE JUSTIFIED AS A NONSTANDARD FEATURE. SEE NOTE 7 ON THIS SHEET.
- RAMP SIDE OPTIONS ARE DETAILED ON SHEET 3 OF 12, WHERE A PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP. FLARED SIDES SHALL BE INSTALLED WITH A MAXIMUM SLOPE OF 9.5% FOR DESIGN AND LAYOUT, AND 10% MAXIMUM FOR WORK ACCEPTANCE. A PEDESTRIAN CIRCULATION PATH IS ASSUMED TO CROSS THE CURB RAMP WHEN AREA ADJACENT TO THE RAMP IS PAVED AND FREE OF VERTICAL OBSTRUCTIONS THAT WOULD PREVENT PEDESTRIAN PASSAGE. THERE IS NO MAXIMUM FLARE SLOPE FOR A RAMP THAT IS NOT CROSSED BY A PEDESTRIAN CIRCULATION PATH.
- THE BACK SIDE OF A PARALLEL RAMP SHOULD BE GRADED TO A MAXIMUM SLOPE OF 25% TO MATCH EXISTING TERRAIN UNLESS OTHERWISE SHOWN IN THE CONTRACT DOCUMENTS. WHERE GRADING IS NOT FEASIBLE DUE TO LIMITED ROW OR PHYSICAL CONSTRAINTS, A BACK CURB MAY BE INSTALLED. SEE DETAILS ON SHEET 3 OF 12 AND SHEET 9 OF 12.
- THE DEPARTMENT'S PREFERENCE IS TO INSTALL TWO SEPARATE CURB RAMPS AT A STREET CORNER THAT SERVES TWO SEPARATE PEDESTRIAN CROSSINGS, WITH EACH RAMP ALIGNED TO THE CROSSING THAT IT SERVES. WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT SEPARATE RAMPS, A SINGLE CURB RAMP (E.G., A DIAGONAL CURB RAMP) IS PERMITTED TO SERVE BOTH PEDESTRIAN CROSSINGS.
- TURNS AND CLEAR SPACE NOTES:**
- WHERE A CHANGE IN DIRECTION IS REQUIRED TO UTILIZE A CURB RAMP, A TURNING SPACE SHALL BE PROVIDED AT THE BASE OR THE TOP OF CURB RAMP, AS APPLICABLE. TURNING SPACES SHALL BE PERMITTED TO OVERLAP CLEAR SPACES.
- WHERE THERE ARE NO VERTICAL CONSTRAINTS AT THE BACK OF SIDEWALK, E.G., VERTICAL CURBS, BUILDINGS, FENCES) THE TURNING SPACE DIMENSIONS SHALL BE 4'-0" X 4'-0" MINIMUM WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, THE TURNING SPACE SHALL BE 4'-0" X 5'-0" MINIMUM. THE 5'-0" DIMENSION SHALL BE IN THE DIRECTION OF THE RAMP RUN.
- TURNING SPACES SHALL NOT BE DESIGNED WITH A SLOPE GREATER THAN 1.5% IN ANY DIRECTION, WHILE PROVIDING POSITIVE DRAINAGE. THE MAXIMUM SLOPE FOR WORK ACCEPTANCE IS 2.0%. THE FOLLOWING EXCEPTIONS ARE ALLOWED:
- WHERE PEDESTRIAN STREET CROSSINGS ARE PROVIDED AT INTERSECTIONS WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE, AND AT MIDBLOCK CROSSINGS, THE CROSS SLOPE OF THE TURNING SPACE SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE. WHEN A RAMP EXISTS BETWEEN THE TURNING SPACE AND THE CURB, THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.
- BELOW THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE OF 4'-0" X 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.
- BELOW THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE OF 4'-0" X 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.
- BELOW THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE OF 4'-0" X 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CROSS SLOPE OF THE TURNING SPACE SHOULD BE LESS STEEP THAN THE ROADWAY GRADE AND AS FLAT AS PRACTICABLE WHENEVER POSSIBLE.

**DEFINITION OF TERMS:**  
ACCESSIBLE ROUTE: SEE "PEDESTRIAN ACCESS ROUTE", BELOW.  
CLEAR SPACE: AN UNOBSTRUCTED FLOOR OR GROUND SPACE THAT WILL ACCOMMODATE A SINGLE, STATIONARY WHEELCHAIR AND OCCUPANT.  
CROSS SLOPE: THE GRADE THAT IS PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL. IN REGARDS TO NOTES 21 AND 22, CROSS SLOPE PERMITTED TO EQUAL STREET OR HIGHWAY GRADE IS THE SLOPE PARALLEL TO THE HIGHWAY. THE CROSS SLOPE PERPENDICULAR TO THE HIGHWAY SHALL BE 1.5% MAXIMUM FOR DESIGN AND 2% MAXIMUM FOR WORK ACCEPTANCE.

**PARALLEL CURB RAMP:** A CURB RAMP WITH THE RAMP SLOPE ORIENTED PARALLEL TO THE CURB OR EDGE OF PAVEMENT.  
PEDESTRIAN ACCESS ROUTE: A CONTINUOUS AND UNOBSTRUCTED PATH OF TRAVEL PROVIDED FOR PEDESTRIANS WITH DISABILITIES WITHIN OR ADJACENT TO A PEDESTRIAN CIRCULATION PATH.  
PEDESTRIAN CIRCULATION PATH: A PREPARED EXTERIOR OR INTERIOR SURFACE PROVIDED FOR PEDESTRIAN TRAVEL IN THE PUBLIC RIGHT-OF-WAY.  
PERPENDICULAR CURB RAMP: A CURB RAMP WITH THE RAMP SLOPE ORIENTED PERPENDICULAR TO THE CURB OR EDGE OF PAVEMENT.  
RUNNING SLOPE: THE GRADE THAT IS PARALLEL TO THE DIRECTION OF PEDESTRIAN TRAVEL.  
STOP-OR-YIELD-CONTROLLED LOCATION: AN INTERSECTION, DRIVEWAY OR PEDESTRIAN CROSSING WHERE VEHICULAR TRAFFIC IS CONTROLLED BY A YIELD SIGN, A STOP SIGN, OR A TRAFFIC SIGNAL THAT FLASHES RED. VEHICULAR TRAFFIC DOES NOT PASS THROUGH A STOP-OR-YIELD-CONTROLLED LOCATION WITHOUT STOPPING OR SLOWING.  
TRAFFIC SIGNAL: THAT IS DESIGNED FOR THE GREEN PHASE. A TRAFFIC SIGNAL OTHER THAN A FLASHING RED OR FLASHING YELLOW.  
TURNING SPACE: A RELATIVELY LEVEL SPACE PROVIDED WHERE A TURNING MANUEVER IS REQUIRED FOR A PEDESTRIAN TO OBTAIN TO A CURB RAMP OR STREET CROSSING.

**DETAIL "A" ACCESSIBLE PASSING SPACE TRANSITION ONE SIDE TAPER**  
**DETAIL "B" ACCESSIBLE PASSING SPACE TRANSITION BOTH SIDES TAPER**

NEW YORK STATE OF OPPORTUNITY Department of Transportation  
U.S. CUSTOMARY STANDARD SHEET  
PEDESTRIAN FACILITIES SHEET 1 OF 12  
APPROVED AUGUST 18, 2021 ISSUED UNDER EI 21-024  
/S/ RICHARD WILDER, P.E. DEPUTY CHIEF ENGINEER DESIGN 608-01

**TRUNCATED DOME DIMENSIONS**

| DIM. | MIN. DIM.           | MAX. DIM. |
|------|---------------------|-----------|
| A    | 1.6"                | 2.4"      |
| B    | 0.85"               | 1.2"      |
| C    | 50% - 65% OF D DIM. |           |
| D    | 0.3"                | 1.4"      |

**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 1**  
GRADE BREAK AT BACK OF CURB GREATER THAN 5'-0"  
GRADE BREAK AT BOTTOM OF RAMP  
CURET (TYP.)  
24" MIN. IN DIRECTION OF TRAVEL (SEE NOTE 4)  
DROP CURB  
DETECTABLE WARNING SURFACE LOCATED ABOVE LOWER GRADE BREAK (SEE NOTES 5(a) AND 5)

**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 4**  
GRADE BREAK AT BACK OF CURB GREATER THAN 5'-0"  
GRADE BREAK AT BOTTOM OF RAMP  
RECTANGULAR UNITS CUT TO FIT DETECTABLE WARNING SURFACE (TYP.)  
CURET (TYP.)  
2" MAX. DISTANCE FROM FRONT CORNER OF DETECTABLE WARNING FIELD TO BACK OF CURB (TYP.) (SEE NOTE 5(a))  
2" MAX. DISTANCE FROM FRONT CORNER OF DETECTABLE WARNING FIELD TO BACK OF CURB (TYP.) (SEE NOTE 5(a))  
DROP CURB

**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 5**  
GRADE BREAK AT BACK OF CURB GREATER THAN 5'-0"  
GRADE BREAK AT BOTTOM OF RAMP  
CURET (TYP.)  
24" MIN. IN DIRECTION OF TRAVEL (SEE NOTE 4)  
DROP CURB  
DETECTABLE WARNING SURFACE LOCATED ABOVE LOWER GRADE BREAK (SEE NOTES 5(a) AND 5))  
SPECIAL ORDER RADIAL UNITS, DETECTABLE WARNING SURFACE (TYP.) (SEE NOTE 5(c))

**DETECTABLE WARNING NOTES:**

- DETECTABLE WARNING SURFACES (DWS) SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS ON PEDESTRIAN ACCESS ROUTES:
- A. CURB RAMPS AND BLENDED TRANSITIONS AT PEDESTRIAN STREET CROSSINGS.
- B. PEDESTRIAN REFUGE ISLANDS WHERE THE LENGTH OF THE PEDESTRIAN ACCESS ROUTE ACROSS THE REFUGE ISLAND IS GREATER THAN OR EQUAL TO 6%.
- C. PEDESTRIAN AT-GRADE RAIL CROSSINGS NOT LOCATED WITHIN A STREET OR HIGHWAY.
- DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE THE PEDESTRIAN ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, YIELD OR STOP CONTROL. DETECTABLE WARNING SURFACES SHALL NOT BE PROVIDED AT CROSSINGS OF UNCONTROLLED DRIVEWAYS.
- WITH THE EXCEPTION OF THE "DETECTABLE WARNING SURFACE TRUNCATED DOME DETAILS" DETECTABLE WARNING DOMES ON THIS SHEET ARE NOT DETICED TO SCALE.
- DETECTABLE WARNING FIELDS SHALL EXTEND 24" MINIMUM IN THE DIRECTION OF PEDETRIAN TRAVEL ACROSS THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE, EXCLUDING ANY FLARED SIDES.
- SOME DETECTABLE WARNING PRODUCTS REQUIRE A CONCRETE BORDER FOR PROPER INSTALLATION. BORDERS CANNOT BE INCLUDED AS PART OF THE 24" MINIMUM DIMENSION DESCRIBED IN NOTE 4.
- WHEN PLACED AT THE BACK OF CURB, DETECTABLE WARNING FIELDS SHOULD BE PLACED AS CLOSE TO THE BACK OF CURB AS PRACTICABLE. BOTH FRONT CORNERS OF THE DETECTABLE WARNING FIELD SHALL NOT BE LOCATED MORE THAN 2" FROM THE BACK OF CURB. WHERE THE BACK OF THE CURB EDGE IS TOOLED TO PROVIDE A RADIUS, THE BORDER DIMENSION SHALL BE MEASURED FROM THE INSIDE EDGE OF THE CURB RADIUS. WHERE CURB IS NOT USED, THE EDGE OF PAVEMENT SHALL BE SUBSTITUTED FOR THE BACK OF CURB FOR PLACEMENT OF DETECTABLE WARNING FIELDS.
- WHEN PLACED ABOVE THE LOWER GRADE BREAK OF A CURB RAMP, DETECTABLE WARNING UNITS SHOULD BE PLACED AS CLOSE TO THE JOINT AS PRACTICABLE. BOTH FRONT CORNERS OF RECTILINEAR DETECTABLE WARNING FIELDS SHALL NOT BE LOCATED MORE THAN 2" FROM THE JOINT.
- WHEN RADIAL DWS UNITS ARE PLACED AT THE BACK OF CURB, THE FRONT EDGE OF THE DWS FIELD SHOULD BE AS CLOSE AS POSSIBLE TO THE BACK OF CURB. I.E., THE RADII OF THE FRONT OF THE DWS FIELD SHOULD MATCH THE RADII ALONG THE BACK OF CURB AS CLOSELY AS POSSIBLE. THE OUTSIDE CORNERS OF THE DWS FIELD MUST BE LOCATED NO MORE THAN 2 INCHES FROM THE BACK OF CURB.
- ON SLOPES OF 6% OR GREATER, THE ROWS OF DOMES SHALL BE ALIGNED TO BE PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK ON THE RAMP RUN. WHERE DOMES ARE MARKED RADIALLY, THE MAX OFFSET IN DIAMETER AND CENTER-TO-CENTER SPACING WITHIN THE BANKS SPECIFIED ON THIS SHEET, DOME ALIGNMENT THAT IS PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK IS NOT REQUIRED ON SLOPES OF LESS THAN 6%.
- THE DETECTABLE WARNING FIELD SHALL BE THE COLOR SPECIFIED IN THE CONTRACT DOCUMENTS OR MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, DETECTABLE WARNING SURFACES CONTRAST VISUALLY WITH ADJACENT OUTER, STREET OR HIGHWAY, OR PEDESTRIAN ACCESS ROUTE SURFACE, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT.

NEW YORK STATE OF OPPORTUNITY Department of Transportation  
U.S. CUSTOMARY STANDARD SHEET  
PEDESTRIAN FACILITIES SHEET 2 OF 12  
APPROVED AUGUST 18, 2021 ISSUED UNDER EI 21-024  
/S/ RICHARD WILDER, P.E. DEPUTY CHIEF ENGINEER DESIGN 608-01

**RAMP SIDE CONFIGURATIONS**

**RAMP SIDE OPTION A: FLARED CONCRETE**  
**RAMP SIDE OPTION B: GRADED EARTH**  
**RAMP SIDE OPTION C: RETURN CURB**  
**RAMP SIDE OPTION D: UNCURBED INTERSECTION**

**RAMP BACK TREATMENTS**

**RAMP BACK OPTION A: GRADED EARTH AND TURF**  
**RAMP BACK OPTION B: BACK CURB**

NEW YORK STATE OF OPPORTUNITY Department of Transportation  
U.S. CUSTOMARY STANDARD SHEET  
PEDESTRIAN FACILITIES SHEET 3 OF 12  
APPROVED AUGUST 18, 2021 ISSUED UNDER EI 21-024  
/S/ RICHARD WILDER, P.E. DEPUTY CHIEF ENGINEER DESIGN 608-01

**CURB RAMP CONFIGURATIONS**

**CURB RAMP CONFIGURATION TYPE 1**  
**CURB RAMP CONFIGURATION TYPE 2**  
**CURB RAMP CONFIGURATION TYPE 3**  
**CURB RAMP CONFIGURATION TYPE 4**  
**CURB RAMP CONFIGURATION TYPE 5**

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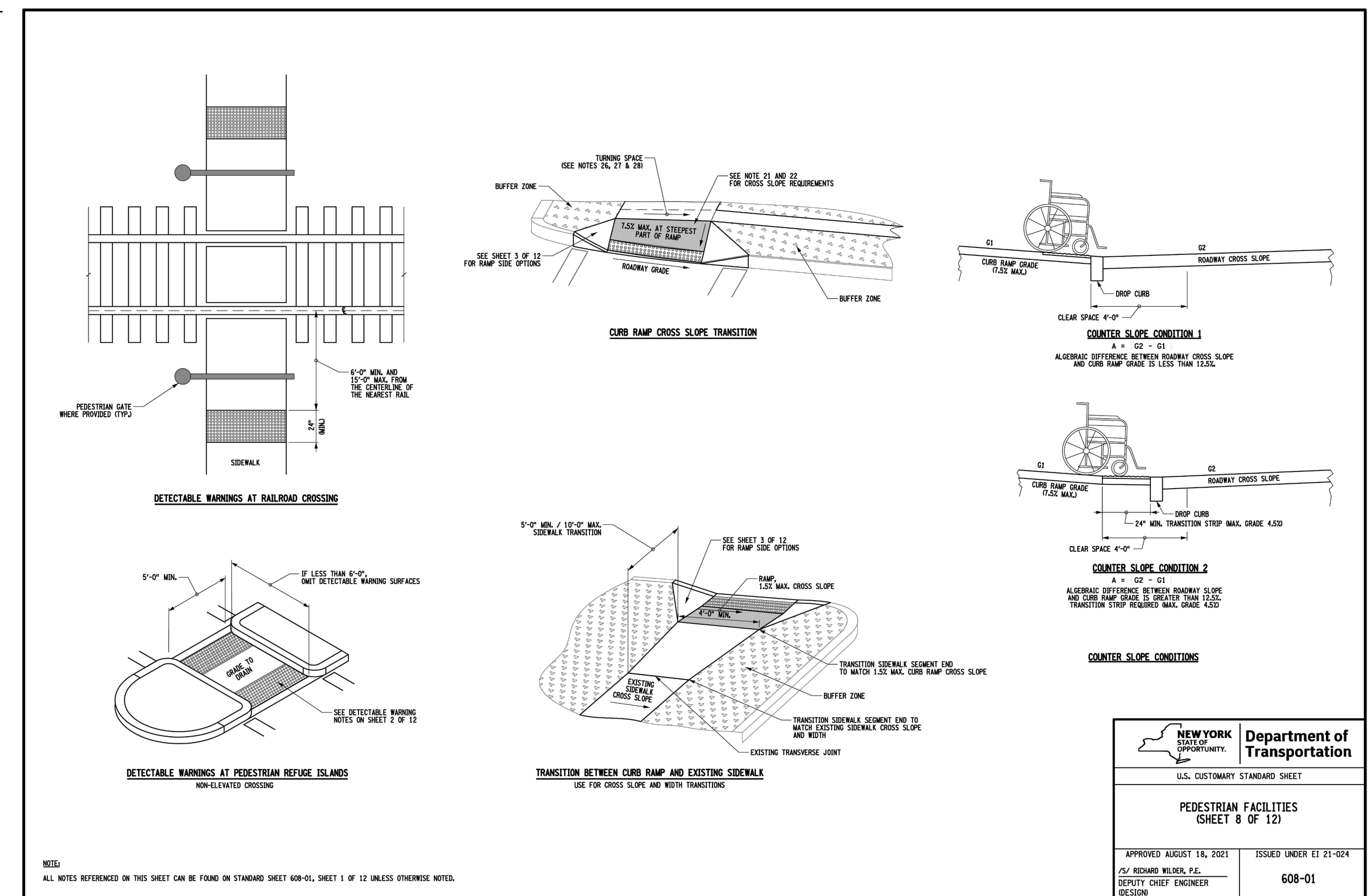
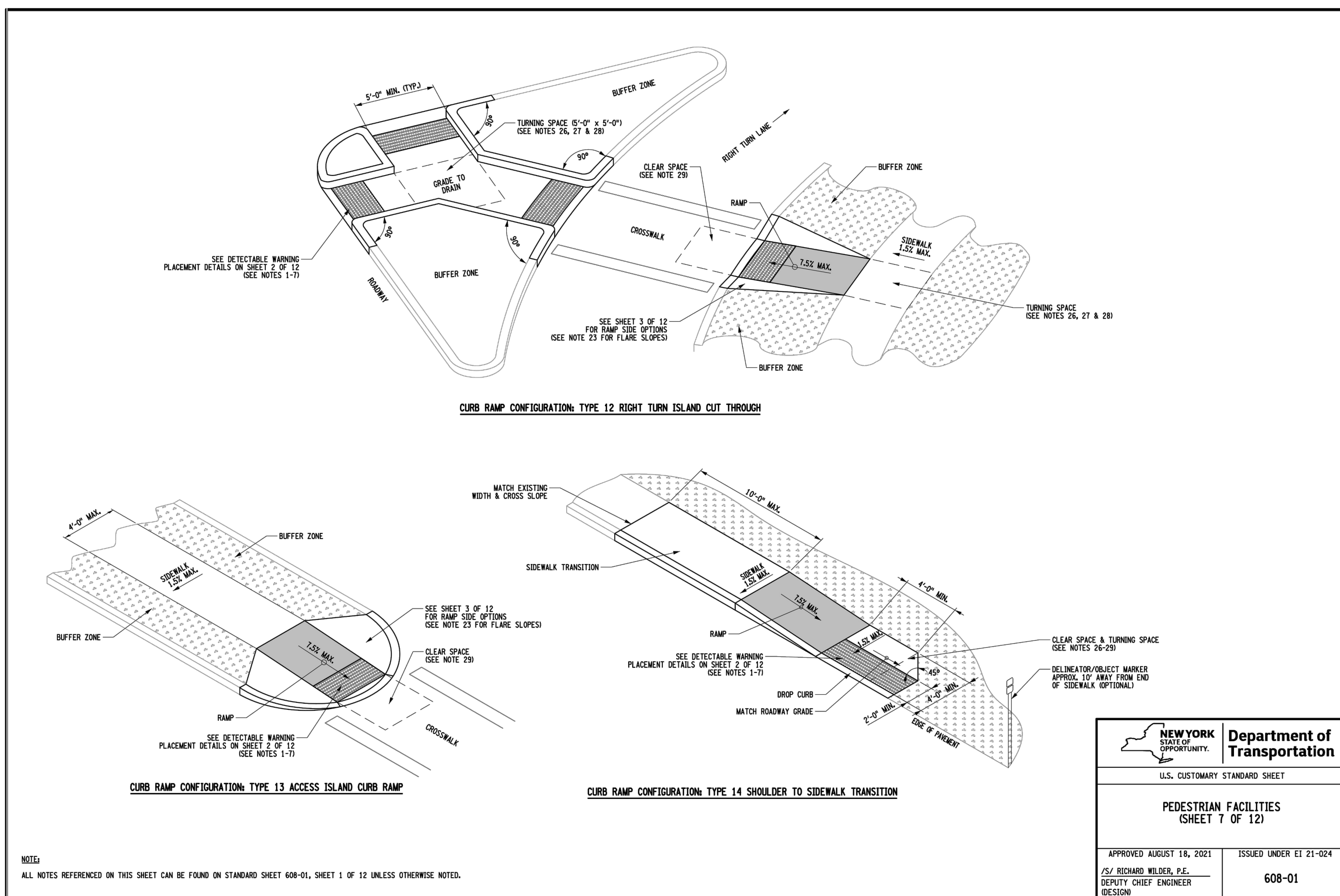
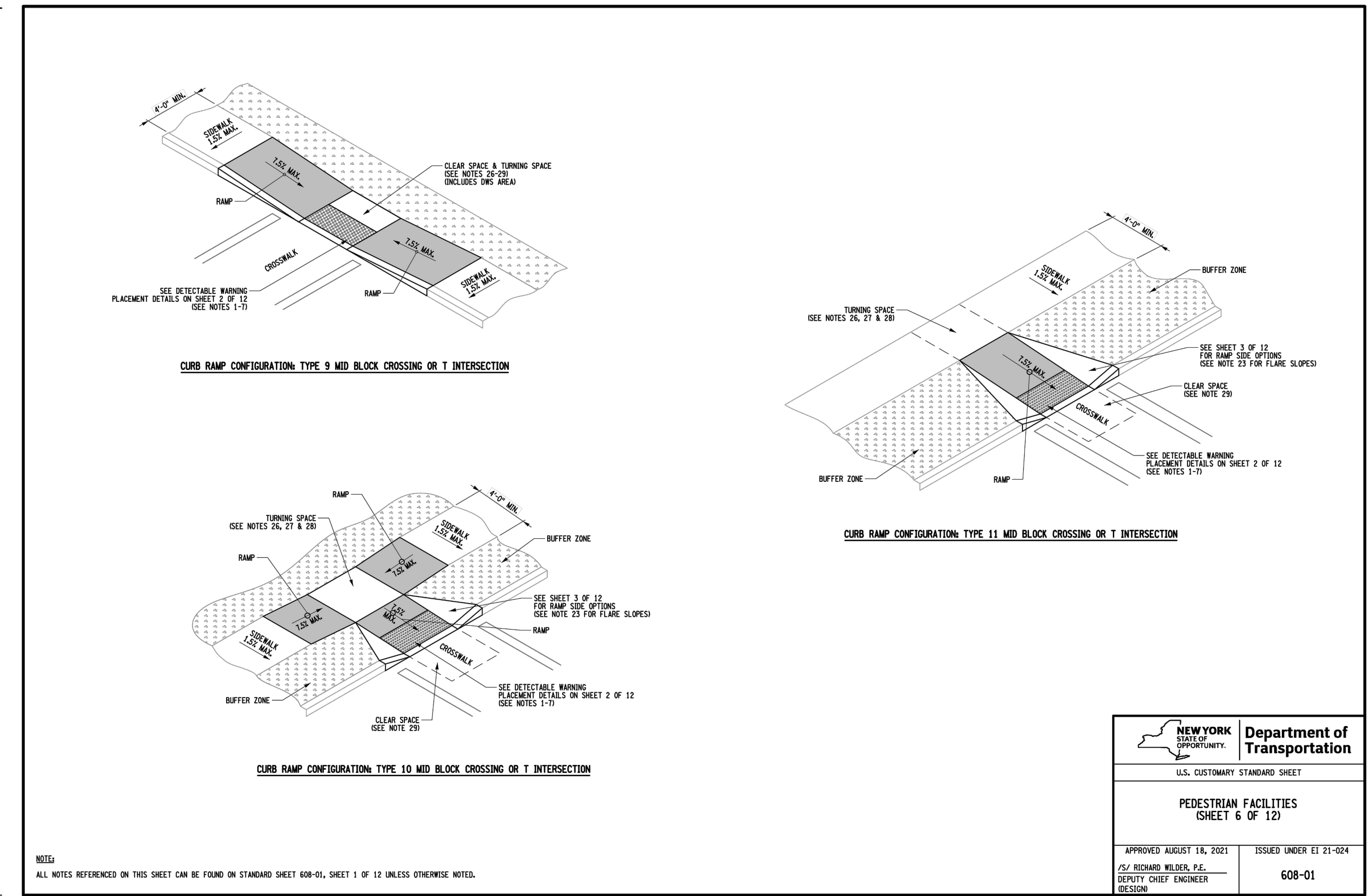
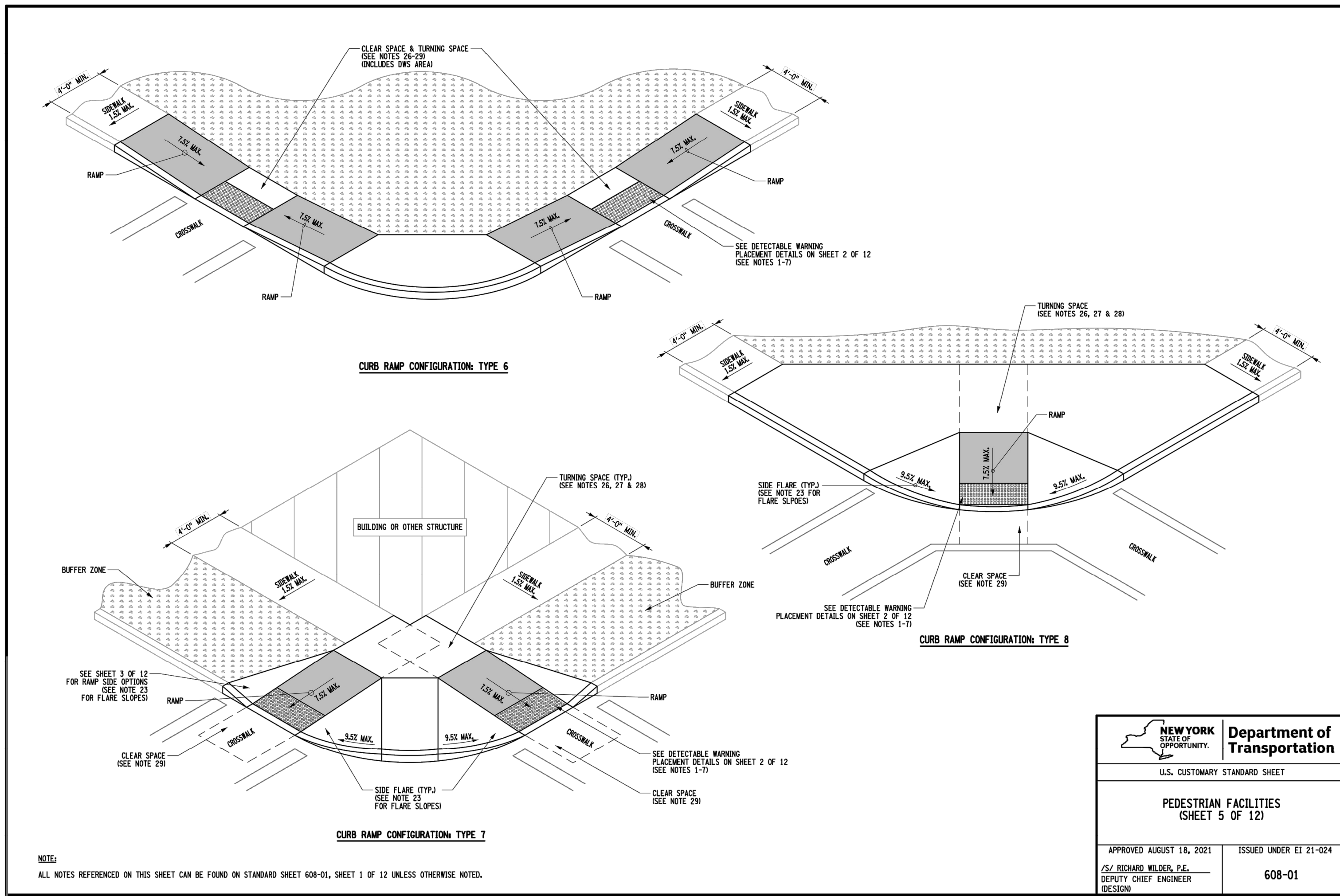
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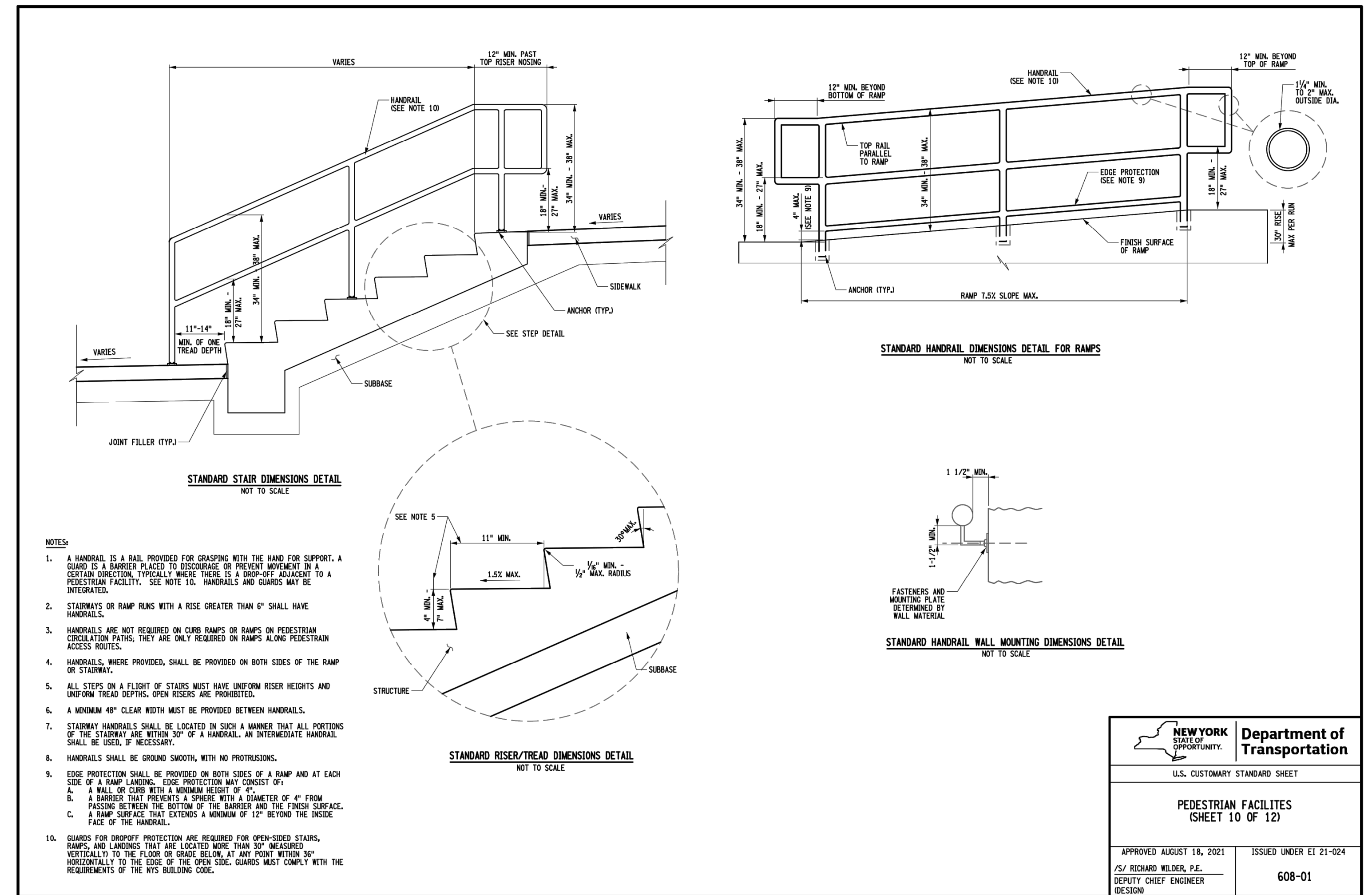
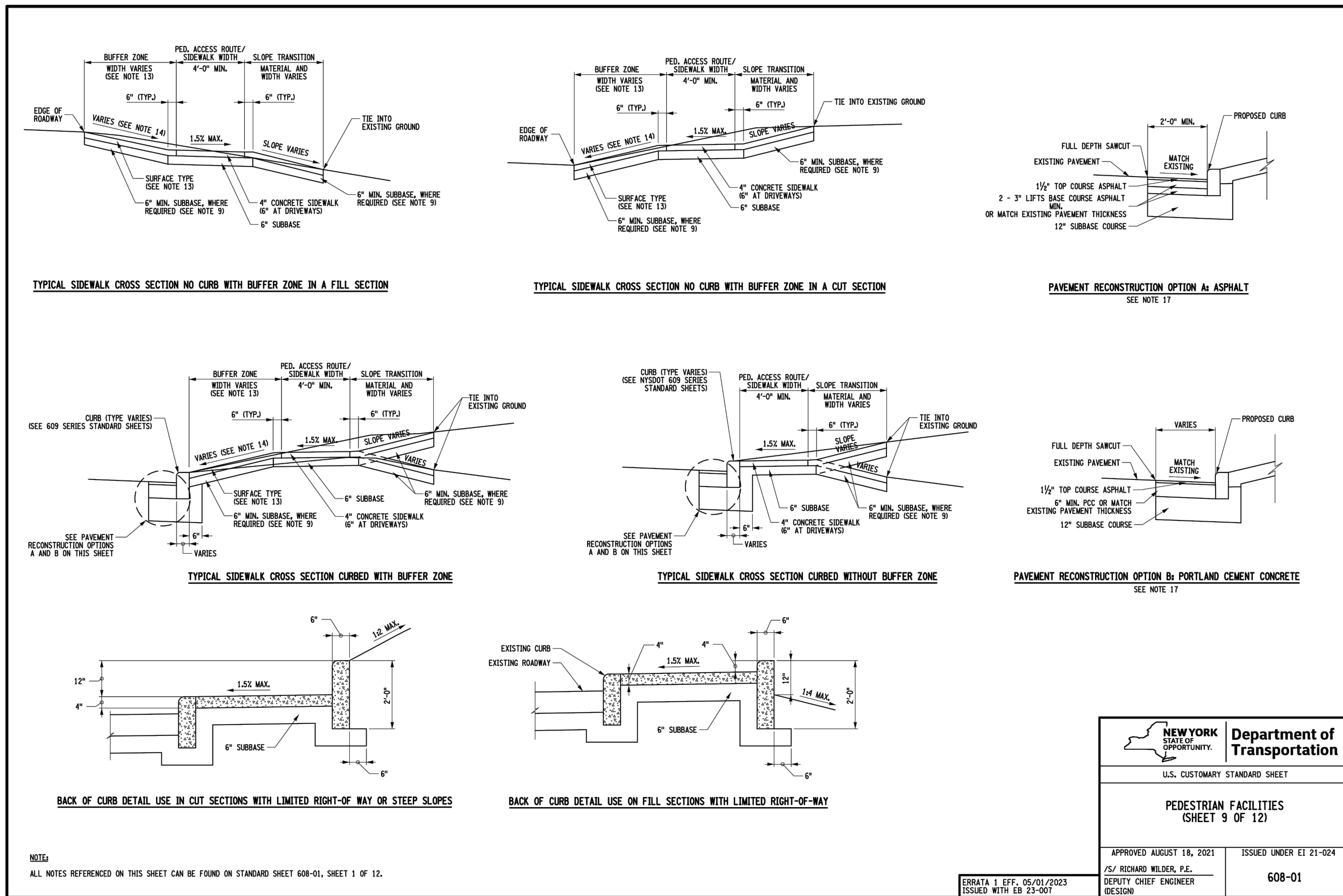
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GENERAL NOTES FOR PEDESTRIAN FACILITIES

- PEDESTRIAN FACILITIES MUST MEET THE VALUES SHOWN ON THE APPLICABLE TABLE, OR BE JUSTIFIED AS NONSTANDARD FEATURES USING EXHIBIT 2-150 FROM THE 2011 ADAAG DESIGN MANUAL, CHAPTER 2.

**TABLE 1 - CRITICAL ELEMENTS FOR THE DESIGN, LAYOUT AND ACCEPTANCE OF PEDESTRIAN FACILITIES, EXISTING CURB RAMPS TO REMAIN ON IR PROJECTS (1991 ADAAG STANDARDS)**

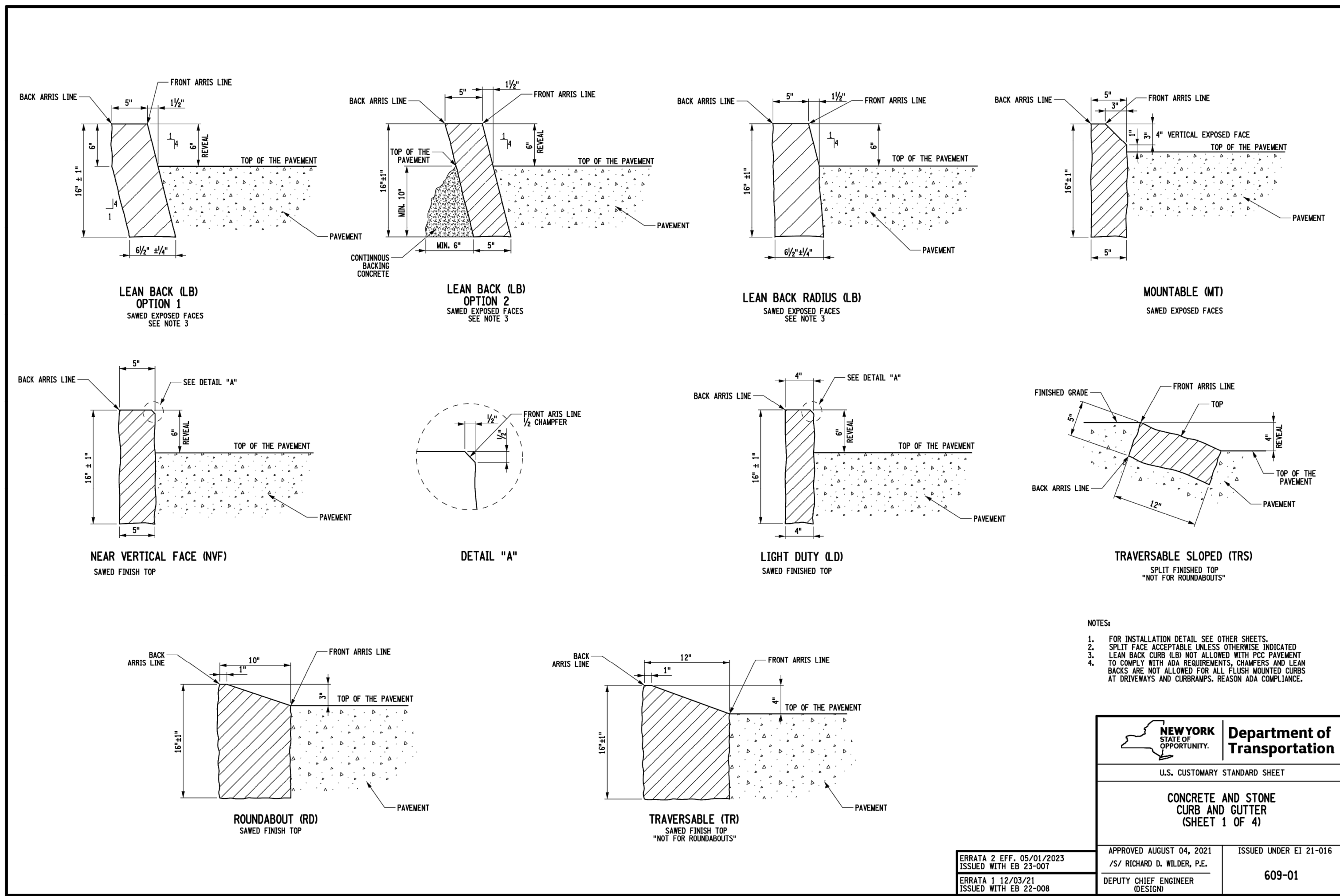
| ELEMENT                                                                                                                               | LIMITS REQUIRED TO REMAIN EXISTING CURB RAMP (SEE NOTE 1)  | INSPECTION METHODS (NOTE NO.) |
|---------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------|-------------------------------|
| <b>CURB RAMPS</b>                                                                                                                     |                                                            |                               |
| FLARE SLOPE FOR RAMP                                                                                                                  | 38° MIN.                                                   | 12                            |
| WITHIN A PEDESTRIAN CIRCULATION PATH                                                                                                  | 10% MAX.                                                   | 10                            |
| FLARE SLOPE/EDGE OF RAMP OUTSIDE A PEDESTRIAN CIRCULATION PATH                                                                        | NO MAX. SLOPE, MAY BE CURBED                               | -                             |
| CROSS SLOPE AT INTERSECTION CROSSING WITH YIELD OR STOP CONTROL                                                                       | 2% MAX.                                                    | 10                            |
| CROSS SLOPE AT CROSSING WITH YIELD OR STOP CONTROL                                                                                    | 1.5% MAX.                                                  | 9                             |
| CROSS SLOPE AT CROSSING WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE | HIGHWAY GRADE IS MAX.                                      | 10                            |
| CROSS SLOPE AT CROSSING WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE | HIGHWAY GRADE IS MAX.                                      | 10                            |
| CROSS SLOPE AT CROSSING WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE | HIGHWAY GRADE IS MAX.                                      | 10                            |
| CROSS SLOPE OF TURNING SPACE AT UNCONTROLLED INTERSECTIONS, IN ANY DIRECTION                                                          | 4.5% MAX.                                                  | 9                             |
| CROSS SLOPE OF TURNING SPACE AT CONTROLLED INTERSECTIONS, IN ANY DIRECTION                                                            | 10% FOR 4" RISE OR LIMITED                                 | 12, 13                        |
| VERTICAL CHANGES                                                                                                                      | 0.5" MAX. WITH 1/2" MAX. BEVEL BETWEEN 0.25" AND 0.5" HIGH | 14                            |

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**TABLE 2 - SELECT CRITICAL ELEMENTS FOR THE DESIGN, LAYOUT AND ACCEPTANCE OF PEDESTRIAN FACILITIES, NEW OR REPLACEMENT FACILITIES (2013 PROWAG STANDARDS)**

| ELEMENT                                                                                                                               | LIMITS FOR DESIGN AND LAYOUT                                                   | LIMITS FOR WORK ACCEPTANCE                                                              | INSPECTION METHODS (NOTE NO.) | ELEMENT                                                                                                                                           | LIMITS FOR DESIGN AND LAYOUT                                                       | LIMITS FOR WORK ACCEPTANCE                                                         | INSPECTION METHODS (NOTE NO.) |
|---------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------|-------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------|------------------------------------------------------------------------------------|-------------------------------|
| <b>CURB RAMPS/BLENDED TRANSITIONS</b>                                                                                                 |                                                                                |                                                                                         |                               | <b>CROSSWALKS</b>                                                                                                                                 |                                                                                    |                                                                                    |                               |
| CLEAR WIDTH                                                                                                                           | 48" MIN.                                                                       | 48" MIN.                                                                                | 12                            | CROSS SLOPE AT INTERSECTION WITH YIELD OR STOP CONTROL                                                                                            | 1.5% MAX.                                                                          | 2% MAX.                                                                            | 10                            |
| CLEAR WIDTH OF FLANDED SIDES, WITHIN PEDESTRIAN CIRCULATION PATH                                                                      | 5.5% MAX.                                                                      | 10% MAX.                                                                                | 10                            | CROSS SLOPE AT PEDESTRIAN CROSSINGS WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE | 4.5% MAX.                                                                          | 5% MAX.                                                                            | 9                             |
| CROSS SLOPE OF FLANDED SIDES, OUTSIDE PEDESTRIAN CIRCULATION PATH                                                                     | NO MAX. SLOPE, MAY BE CURBED                                                   | NO MAX. SLOPE, MAY BE CURBED                                                            | 10                            | CROSS SLOPE, MIDBLOCK                                                                                                                             | HIGHWAY GRADE IS MAX.                                                              | HIGHWAY GRADE IS MAX.                                                              | 10                            |
| GRADE RUNNING SLOPE FOR CURB RAMP                                                                                                     | 7.5% MAX.                                                                      | 8.3% MAX.                                                                               | 9                             | CROSS SLOPE, SIDEWALK                                                                                                                             | 4.5% MAX.                                                                          | 5% MAX.                                                                            | 9                             |
| GRADE RUNNING SLOPE FOR BLENDED TRANSITION                                                                                            | 4.5% MAX.                                                                      | 5% MAX.                                                                                 | 9                             | CROSS SLOPE AT TOP AND BOTTOM OF STAIRS                                                                                                           | 11" MIN.                                                                           | 11" MIN.                                                                           | 12                            |
| CROSS SLOPE AT CROSSING WITH YIELD OR STOP CONTROL                                                                                    | 1.5% MAX.                                                                      | 2% MAX.                                                                                 | 10                            | CROSS SLOPE AT TOP AND BOTTOM OF STAIRS                                                                                                           | LEVEL TO 1.5% MAX.                                                                 | LEVEL TO 2% MAX.                                                                   | 11                            |
| CROSS SLOPE AT CROSSING WHERE THERE IS NO YIELD OR STOP SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE | HIGHWAY GRADE IS MAX.                                                          | HIGHWAY GRADE IS MAX.                                                                   | 10                            | TREAD SURFACE SLOPE                                                                                                                               | LEVEL TO 1.5% MAX. ON LEADING EDGE, 1.25% MAX. EXTENSION OVER TREAD BELOW          | LEVEL TO 2% MAX. ON LEADING EDGE, 1.25% MAX. EXTENSION OVER TREAD BELOW            | 13                            |
| LENGTH OF CURB RAMP, IF MAXIMUM ALLOWABLE SLOPE WILL NOT "CATCH UP" TO GRADE                                                          | DOES NOT NEED TO EXCEED 15'-1"                                                 | DOES NOT NEED TO EXCEED 15'                                                             | 13                            | TREAD NOSING                                                                                                                                      | YES                                                                                | YES                                                                                | -                             |
| TURNING SPACE, WITH NO CONSTRAINTS                                                                                                    | 48"x48" MIN.                                                                   | 48"x48" MIN.                                                                            | 12, 13                        | CLOSED RISER                                                                                                                                      | YES                                                                                | YES                                                                                | -                             |
| TURNING SPACE, WITH CONSTRAINTS AT BACK OF SIDEWALK                                                                                   | 48"x60" MIN.                                                                   | 48"x60" MIN.                                                                            | 12, 13                        | RISE BETWEEN LANDINGS                                                                                                                             | 144" MAX.                                                                          | 144" MAX.                                                                          | 14                            |
| TURNING SPACE, WITH CONSTRAINTS ON TWO SIDES                                                                                          | 48"x60" MIN.                                                                   | 48"x60" MIN.                                                                            | 12, 13                        | LANDING                                                                                                                                           | REQ. AT TOP AND BOTTOM OF STAIRS                                                   | REQ. AT TOP AND BOTTOM OF STAIRS                                                   | 11                            |
| CROSS SLOPE OF TURNING SPACE AT CONTROLLED INTERSECTIONS, IN ANY DIRECTION                                                            | 1.5% MAX.                                                                      | 2% MAX.                                                                                 | 11                            | LANDING WIDTH                                                                                                                                     | MIN. WIDTH EQUAL TO STAIRWAY WIDTH                                                 | MIN. WIDTH EQUAL TO STAIRWAY WIDTH                                                 | 12                            |
| CROSS SLOPE OF TURNING SPACE AT UNCONTROLLED INTERSECTIONS, IN ANY DIRECTION                                                          | HIGHWAY GRADE IS MAX.                                                          | HIGHWAY GRADE IS MAX.                                                                   | 10                            | LANDING LENGTH                                                                                                                                    | MIN. LENGTH EQUAL TO STAIRWAY WIDTH, UP TO 48"                                     | MIN. LENGTH EQUAL TO STAIRWAY WIDTH, UP TO 48"                                     | 13                            |
| CROSS SLOPE OF TURNING SPACE AT MIDBLOCK CROSSINGS                                                                                    | HIGHWAY GRADE IS MAX.                                                          | HIGHWAY GRADE IS MAX.                                                                   | 10                            | LANDING SLOPE, IN ANY DIRECTION                                                                                                                   | 1.5% MAX.                                                                          | 2% MAX.                                                                            | 11                            |
| CLEAR SPACE AT BOTTOM OF RAMP                                                                                                         | 4.5% MAX.                                                                      | 5% MAX.                                                                                 | 9                             | <b>LANDINGS</b>                                                                                                                                   |                                                                                    |                                                                                    |                               |
| CLEAR SPACE BEYOND BOTTOM GRADE BREAK, OUTSIDE OF PARALLEL VEHICLE PATHS (CAN INCLUDE DROP CURB)                                      | 48"x48" MIN.                                                                   | 48"x48" MIN.                                                                            | 12, 13                        | DRAINAGE                                                                                                                                          | NO LOW SPOTS THAT WILL POOD WATER WITHIN PWD. ACCESS ROUTE                         | NO LOW SPOTS THAT WILL POOD WATER WITHIN PWD. ACCESS ROUTE                         | -                             |
| <b>INTERMEDIATE WALKWAYS</b>                                                                                                          |                                                                                |                                                                                         |                               | <b>PEDESTRIAN SIGNALS</b>                                                                                                                         |                                                                                    |                                                                                    |                               |
| TRANSVERSE AND SPACING                                                                                                                | ON DOT APPROVED LIST                                                           | ON DOT APPROVED LIST                                                                    | -                             | PUSH BUTTON HEIGHT                                                                                                                                | 42" MIN. - 48" MAX.                                                                | 42" MIN. - 48" MAX.                                                                | 17                            |
| CONTRAST OF WARNING DEVICE                                                                                                            | LIGHT ON DARK OR DARK ON LIGHT                                                 | LIGHT ON DARK OR DARK ON LIGHT                                                          | -                             | PUSH BUTTON DISTANCE FROM PEDESTRIAN ACCESS ROUTE                                                                                                 | 9" MAX.                                                                            | 10" MAX.                                                                           | 12                            |
| ALIGNMENT ON SLOPES > 5%                                                                                                              | PERPENDICULAR TO GRADE BREAK BETWEEN RAMP RUN AND STREET                       | PERPENDICULAR TO GRADE BREAK BETWEEN RAMP RUN AND STREET                                | 12                            | TRANSVERSE OF CLEAR SPACE ADJACENT TO PUSH BUTTON                                                                                                 | 30"x48" MIN.                                                                       | 30"x48" MIN.                                                                       | 12, 13                        |
| WIDTH                                                                                                                                 | FULL WIDTH OF RAMP OR PAR (2" BORDER ALLOWED)                                  | FULL WIDTH OF RAMP OR PAR (2" BORDER ALLOWED)                                           | 12                            | GRADE RUNNING SLOPE OF CLEAR SPACE ADJACENT TO PUSH BUTTON                                                                                        | WATER GRADE OF ADJACENT PWD. ACCESS ROUTE                                          | WATER GRADE OF ADJACENT PWD. ACCESS ROUTE                                          | -                             |
| LENGTH DEPTH                                                                                                                          | 24" MIN. IN DIRECTION OF PEDESTRIAN TRAVEL ACROSS FULL WIDTH OF RAMP OR P.A.R. | 24" MIN. IN DIRECTION OF PEDESTRIAN TRAVEL ACROSS FULL WIDTH OF SIDEWALK RAMP OR P.A.R. | 13                            | CROSS SLOPE OF CLEAR SPACE ADJACENT TO PUSH BUTTON                                                                                                | 1.5% MAX.                                                                          | 2% MAX.                                                                            | 10                            |
| <b>SIDEWALK (SEE NOTE 2)</b>                                                                                                          |                                                                                |                                                                                         |                               | CLEARANCE TIMING                                                                                                                                  | 3.5 FT/S MAX. WALKING SPEED                                                        | 3.5 FT/S MAX. WALKING SPEED                                                        | -                             |
| CLEAR WIDTH OF PWD. ACCESS ROUTE (EXCLUDING CURBS)                                                                                    | 48" MIN.                                                                       | 48" MIN.                                                                                | 12                            | <b>RAMPS AND GUARDS FOR PEDESTRIAN RAMPS, STAIRS AND ELEVATED SURFACES</b>                                                                        |                                                                                    |                                                                                    |                               |
| CLEAR WIDTH OF SIDEWALK WHERE HWY. GRADE IS 5% OR LESS                                                                                | 4.5% MAX.                                                                      | 5% MAX.                                                                                 | 9                             | RAMP WITH A RISE OF OVER 6" OR STAIRS                                                                                                             | HANDRAIL, REQ. ON BOTH SIDES                                                       | HANDRAIL, REQ. ON BOTH SIDES                                                       | -                             |
| CROSS SLOPE                                                                                                                           | HWY. EDGE OF P.W.M.T. GRADE IS > 5%                                            | HWY. EDGE OF P.W.M.T. GRADE IS > 5%                                                     | 9                             | HEIGHT OF HANDRAIL FROM WALKING SURFACE ON STAIRS NOSING TO TOP OF HEADS OF STAIRS                                                                | 34" MIN. - 38" MAX.                                                                | 34" MIN. - 38" MAX.                                                                | 14                            |
| PASSING SPACE INTERVAL OF PWD. ACCESS ROUTE IS LESS THAN 60" WIDE                                                                     | 200" MAX.                                                                      | 200" MAX.                                                                               | 13                            | HEIGHT OF HANDRAIL FROM WALKING SURFACE ON STAIRS NOSING TO TOP OF HEADS OF STAIRS                                                                | 42" MIN.                                                                           | 42" MIN.                                                                           | 14                            |
| PASSING SPACE DIMENSIONS                                                                                                              | 60"x60" MIN.                                                                   | 60"x60" MIN.                                                                            | 12, 13                        | OPENING LIMITATIONS FOR GUARDS (SEE NOTE 3, THIS SHEET)                                                                                           | 4" MAX. (4 1/8" ALLOWED FROM A HEIGHT OF 36" TO 42")                               | 4" MAX. (4 1/8" ALLOWED FROM A HEIGHT OF 36" TO 42")                               | 15                            |
| <b>SURFACES</b>                                                                                                                       |                                                                                |                                                                                         |                               | DISTANCE BETWEEN BOTTOM OF GUARD AND WALKING SURFACE ON RAMP                                                                                      | 4" MAX.                                                                            | 4" MAX.                                                                            | 15                            |
| HORIZONTAL OPENINGS (SUCH AS GRATINGS AND JOINTS)                                                                                     | 0.5" MAX.                                                                      | 0.5" MAX.                                                                               | 13, 15                        | DISTANCE BETWEEN BOTTOM OF GUARD AND WALKING SURFACE ON STAIRS                                                                                    | TRIANGLE FORMED BY RISER, TREAD, AND BOTTOM RAIL CANNOT ALLOW PASSAGE OF 6" SPHERE | TRIANGLE FORMED BY RISER, TREAD, AND BOTTOM RAIL CANNOT ALLOW PASSAGE OF 6" SPHERE | 15                            |
| VERTICAL DISCONTINUITIES                                                                                                              | 0.25" MAX.                                                                     | BEVEL BETWEEN 0.25" AND 0.5" HIGH                                                       | 14                            | HANDRAIL CLEARANCE BETWEEN SHOULDER SURFACE AND HANDRAIL                                                                                          | 1.5" MIN.                                                                          | 1.5" MIN.                                                                          | 12                            |

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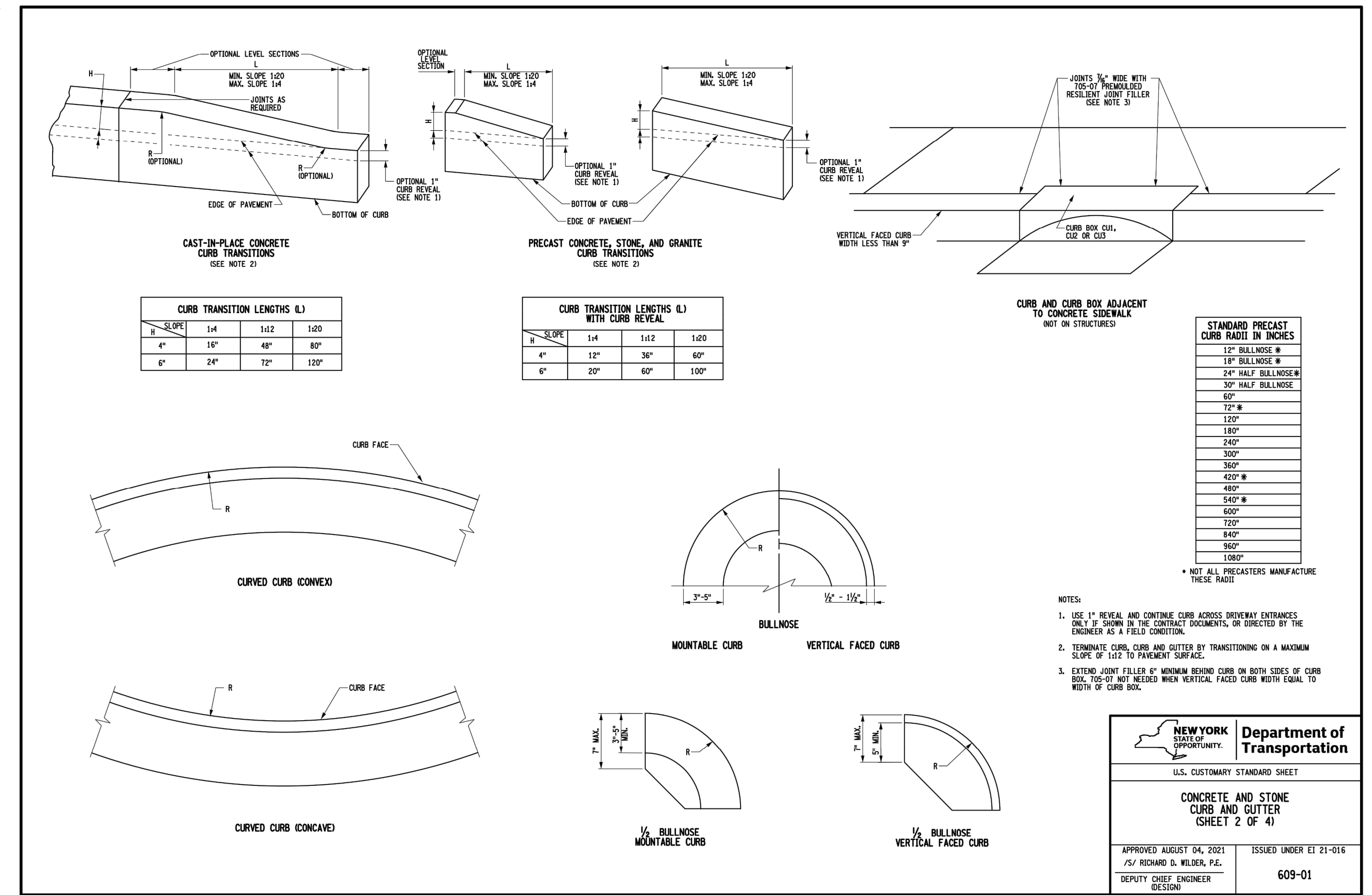
U.S. CUSTOMARY STANDARD SHEET

**CONCRETE AND STONE CURB AND GUTTER (SHEET 1 OF 4)**

APPROVED AUGUST 04, 2021 ISSUED UNDER EI 21-016  
/S/ RICHARD D. WILDER, P.E.  
DEPUTY CHIEF ENGINEER DESIGN 609-01

ERRATA 2 EFF. 05/01/2023  
ISSUED WITH EB 23-007

ERRATA 1 EFF. 12/03/21  
ISSUED WITH EB 22-008



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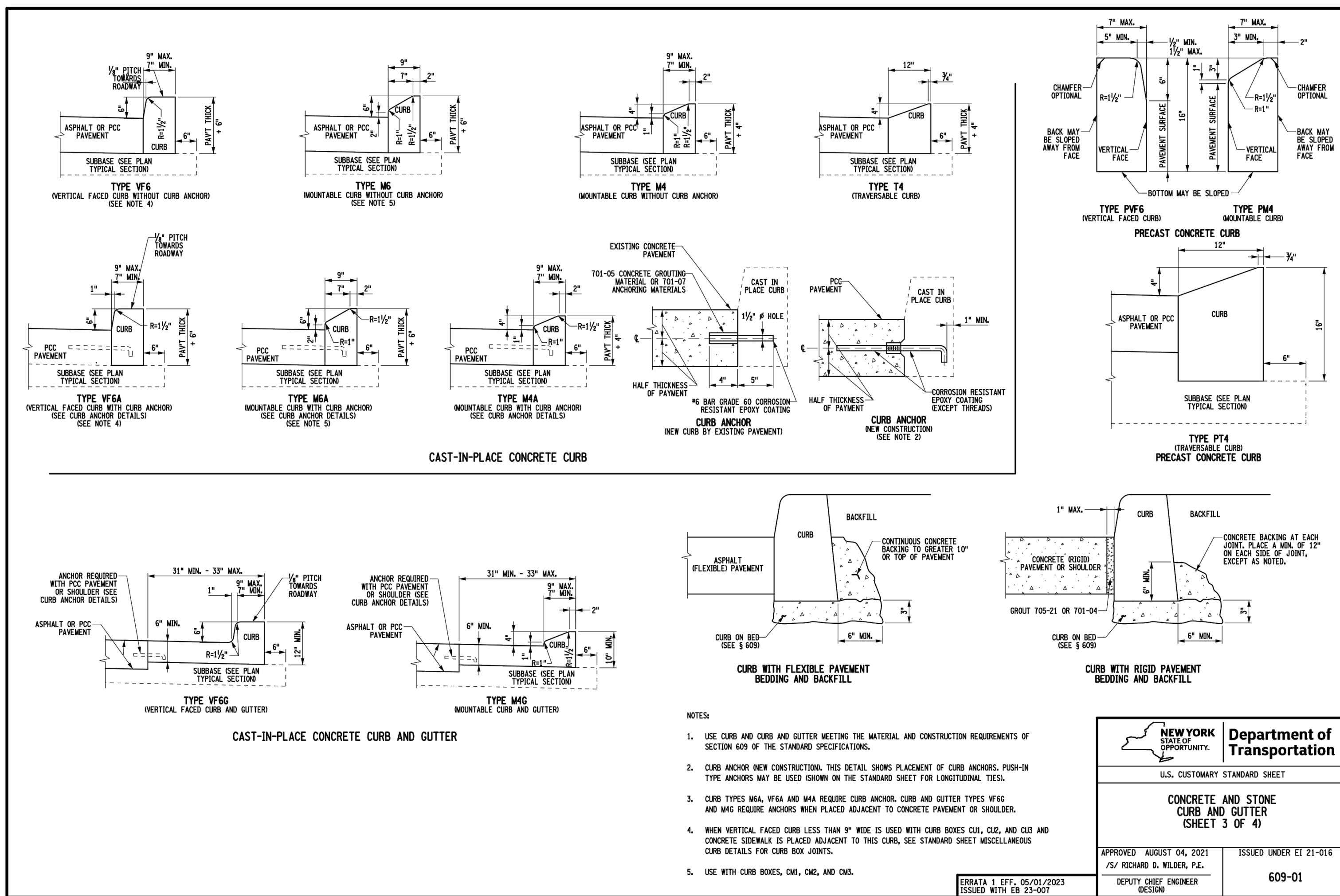
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U.S. CUSTOMARY STANDARD SHEET

**CONCRETE AND STONE CURB AND GUTTER (SHEET 2 OF 4)**

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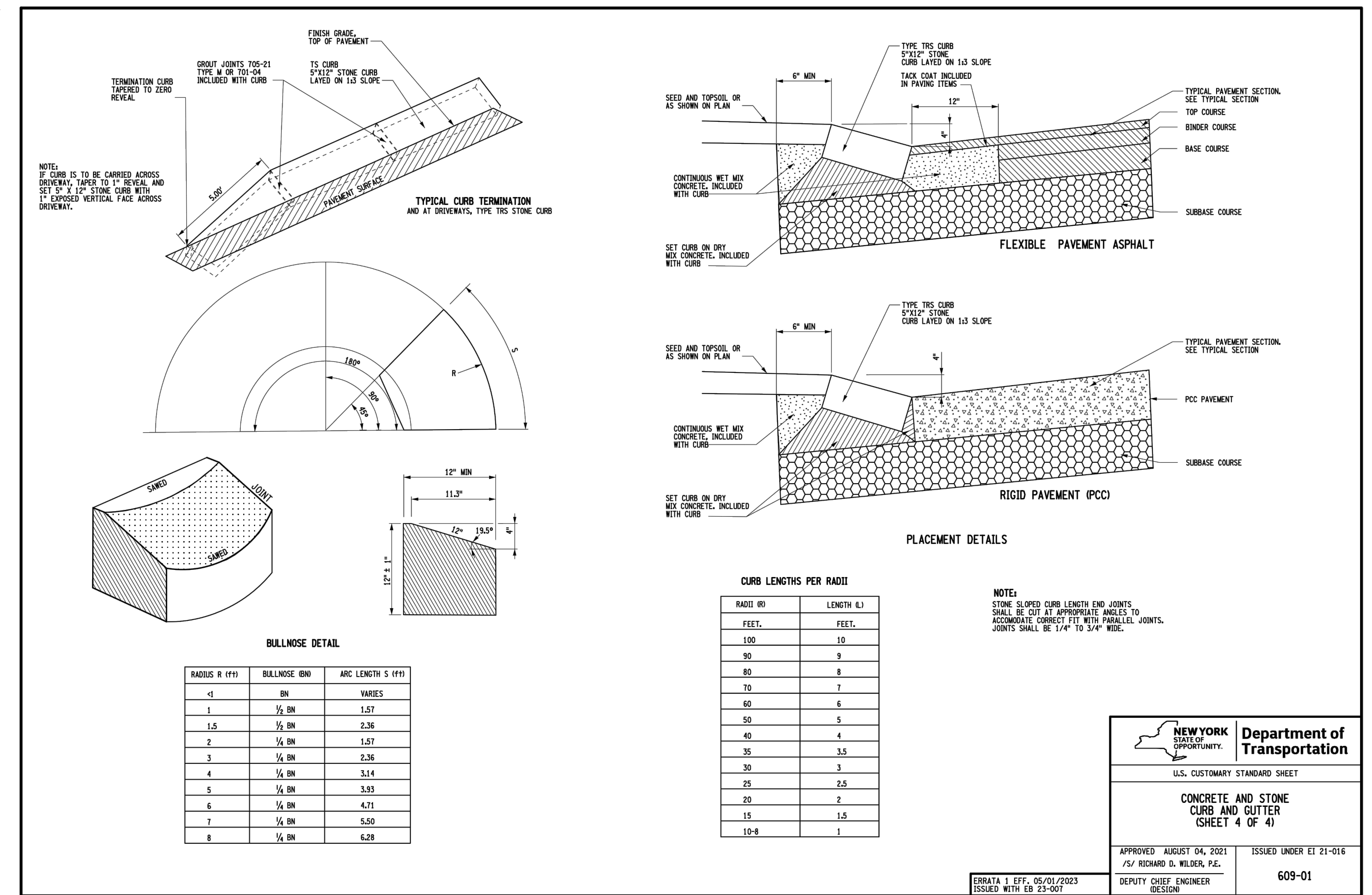
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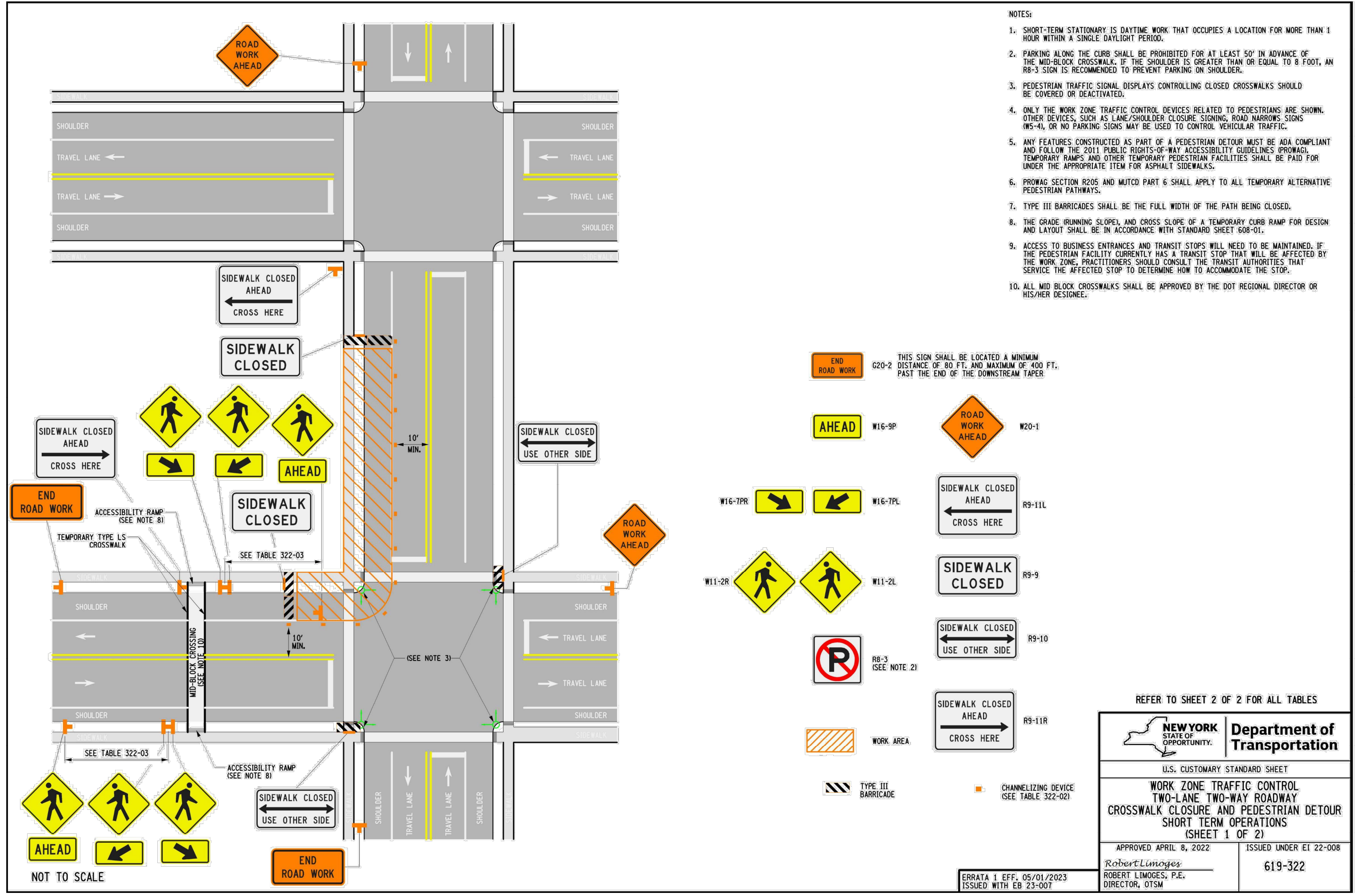
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**TABLE 322-01: REQUIRED SIGN SIZES\***

| SIGN           | NON-FREEMWAY | FREEMWAY |
|----------------|--------------|----------|
| W20-2          | 36x18        | 48x24    |
| R8-3           | 24x24        | 48x48    |
| R9-9**         | 24x12        | 24x12    |
| R9-10**        | 24x12        | 24x12    |
| R9-11/R9-11R** | 24x18        | 24x18    |
| R11-2          | 48x30        | 48x30    |
| W16-9P**       | 24x12        | -        |
| W16-7P**       | 24x12        | -        |
| W11-2R/W11-2L  | 36x36        | -        |
| W20-1          | 36x36        | 48x48    |

\*FREEMWAY SIZES MAY BE USED ON NON-FREEMWAY, IF SPACE CONSTRAINTS DO NOT EXIST.  
 \*\*SIGNS NOT FOR FREEMWAY USE.

**TABLE 322-02: CHANNELIZING DEVICE APPLICATION FOR SHORT-TERM STATIONARY WORK ZONES**

| WORK ZONE PROVISIONS                                                                                                         | MAXIMUM DEVICE SPACING (CENTER TO CENTER) | MOTO COMPLIANT CHANNELIZING DEVICE |            |                  |                 |                              |                 |                           |   |   |
|------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------|------------------------------------|------------|------------------|-----------------|------------------------------|-----------------|---------------------------|---|---|
|                                                                                                                              |                                           | STANDARD CONES                     | TALL CONES | EXTRA TALL CONES | TUBULAR MARKERS | INTERMEDIATE TUBULAR MARKERS | VERTICAL PANELS | OVERSIZED VERTICAL PANELS |   |   |
| SHORT-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD. | 20 FT.                                    | X                                  | X          | X                | X               | -                            | -               | X                         | X | - |

NOTES: X = ALLOWED, BLANK = NOT ALLOWED

**TABLE 322-03: GUIDELINES FOR ADVANCE PLACEMENT OF WARNING SIGNS**

| POSTED SPEED OR 85% PERCENTILE SPEED (MPH) | ADVANCE PLACEMENT DISTANCE (FT.) <sup>1</sup>                                |     |     |     |     |                                                                                             |     |     |     |     |     |
|--------------------------------------------|------------------------------------------------------------------------------|-----|-----|-----|-----|---------------------------------------------------------------------------------------------|-----|-----|-----|-----|-----|
|                                            | CONDITION A: SPEED REDUCTION AND LANE CHANGING IN HEAVY TRAFFIC <sup>2</sup> |     |     |     |     | CONDITION B: DECELERATION TO THE LISTED ADVISORY SPEED (MPH) FOR THE CONDITION <sup>3</sup> |     |     |     |     |     |
|                                            | 5                                                                            | 10  | 15  | 20  | 25  | 30                                                                                          | 35  | 40  | 45  | 50  | 55  |
| 20                                         | 410                                                                          | 315 | 110 | 105 | 80  | 75                                                                                          | -   | -   | -   | -   | -   |
| 25                                         | 515                                                                          | 355 | 160 | 150 | 135 | 120                                                                                         | 95  | -   | -   | -   | -   |
| 30                                         | 620                                                                          | 390 | 205 | 195 | 185 | 165                                                                                         | 140 | 110 | -   | -   | -   |
| 35                                         | 720                                                                          | 350 | 255 | 245 | 235 | 215                                                                                         | 190 | 160 | 130 | -   | -   |
| 40                                         | 825                                                                          | 305 | 320 | 310 | 295 | 280                                                                                         | 255 | 225 | 190 | 150 | -   |
| 45                                         | 930                                                                          | 360 | 380 | 370 | 360 | 340                                                                                         | 315 | 285 | 250 | 210 | -   |
| 50                                         | 1030                                                                         | 425 | 455 | 450 | 435 | 415                                                                                         | 390 | 360 | 330 | 285 | 240 |
| 55                                         | 1135                                                                         | 495 | 530 | 520 | 505 | 490                                                                                         | 460 | 435 | 400 | 355 | 315 |

- NOTES:**
- THE DISTANCES HAVE NOT BEEN MODIFIED TO ACCOUNT FOR SIGN LEGIBILITY.
  - TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST USE EXTRA TIME TO ADJUST SPEED AND CHANGE LANES IN HEAVY TRAFFIC BECAUSE OF A COMPLEX DRIVING SITUATION. TYPICAL SIGNS ARE MERGE AND RIGHT LANE SIGN. THE DISTANCES ARE TAKEN FROM THE 2004 ASHTO POLICY, EXHIBIT 3-3, DECESSION SIGHT DISTANCE, AVOIDANCE MANUEVER E.
  - TYPICAL CONDITION IS THE WARNING OF A POTENTIAL STOP SITUATION. TYPICAL SIGNS ARE STOP AHEAD, YIELD AHEAD SIGNAL AHEAD, AND INTERSECTION WARNING SIGNS.
  - TYPICAL CONDITIONS ARE LOCATIONS WHERE THE ROAD USER MUST DECREASE SPEED TO MANUEVER THROUGH THE WARNED CONDITION. TYPICAL SIGNS ARE TURN, CURVE, REVERSE TURN, OR REVERSE CURVE. THE DISTANCES ARE DETERMINED BY PROVIDING A 2.5 SECOND PRT AND A VEHICLE DECELERATION RATE OF 10 FT./SECOND<sup>2</sup>.
  - TYPICAL SIGNS ARE AHEAD LANE, ROAD NARROWS, DIVIDED HIGHWAY, AND CONDITION B SIGNS DISPLAYED WITHOUT AN ADVISORY SPEED.

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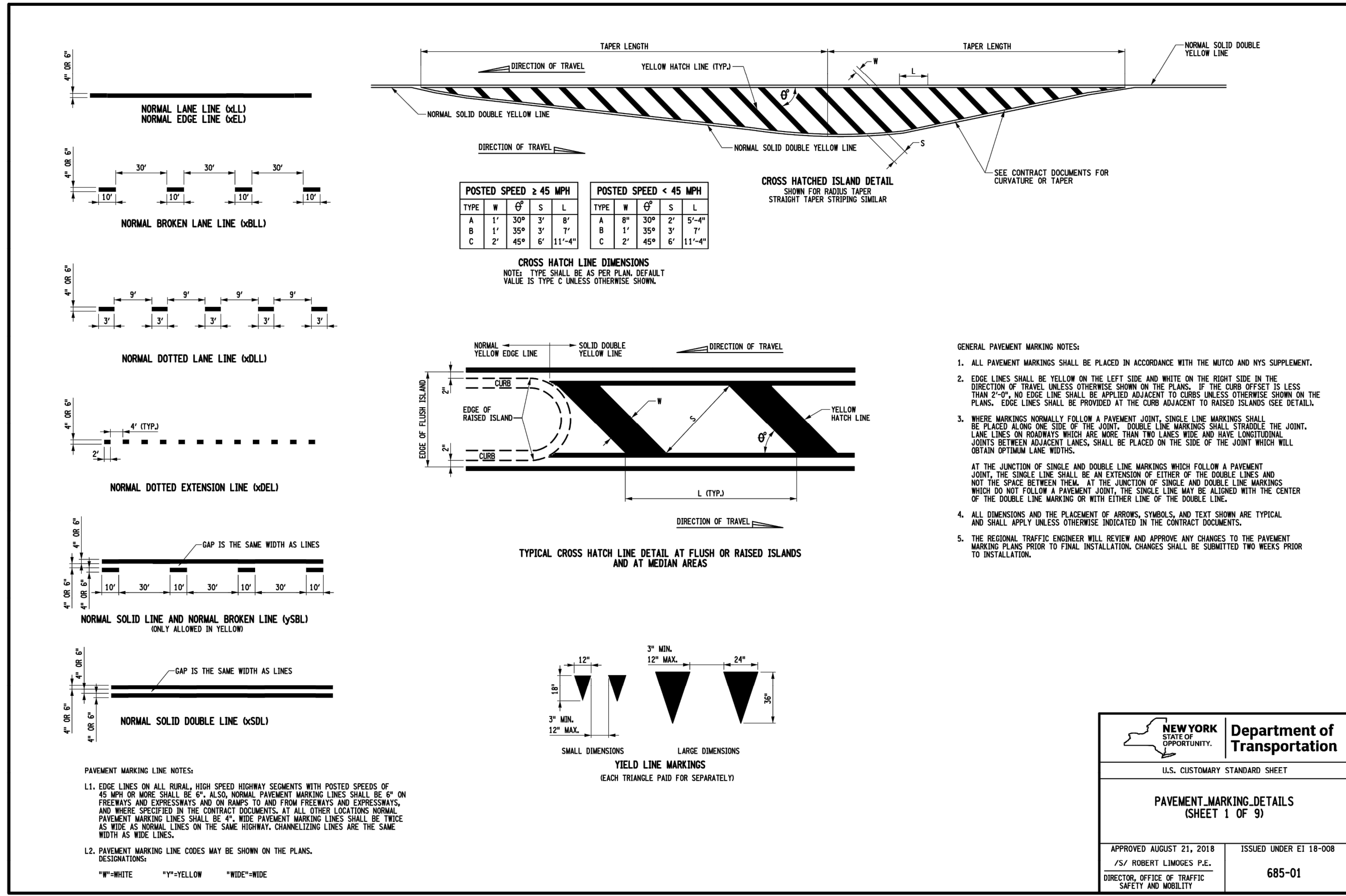
U.S. CUSTOMARY STANDARD SHEET  
**WORK ZONE TRAFFIC CONTROL**  
**TWO-LANE TWO-WAY ROADWAY**  
**CROSSWALK CLOSURE AND PEDESTRIAN DETOUR**  
**SHORT TERM OPERATIONS**  
 (SHEET 2 OF 2)

APPROVED APRIL 8, 2022 ISSUED UNDER ET 22-008  
 Robert Limoges, P.E. DIRECTOR, OTSM  
 Robert Limoges, P.E. DIRECTOR, OTSM

DATE/TIME USER = 23-MAY-2023 11:41 AM

619-322-ET.dwg

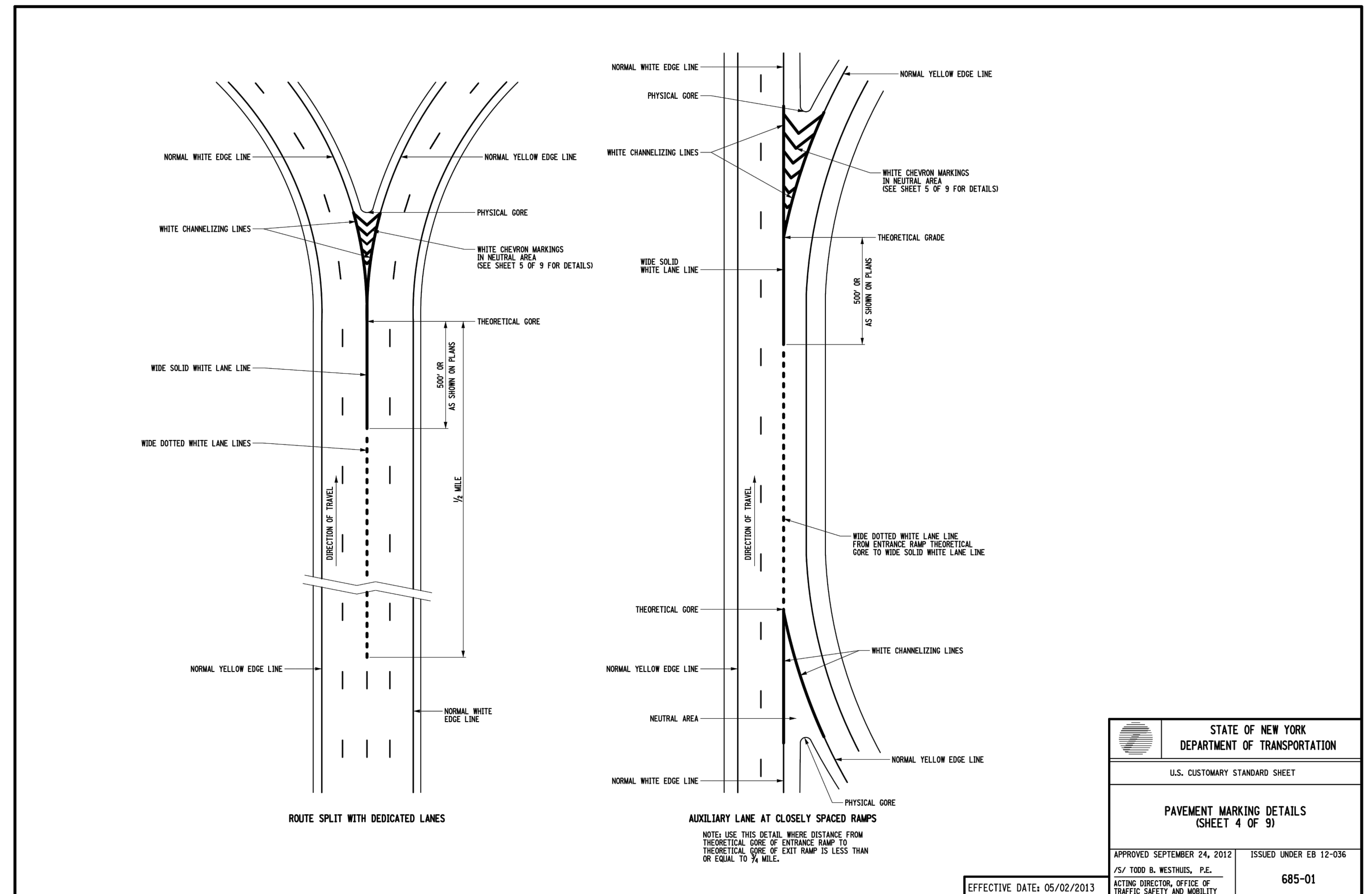
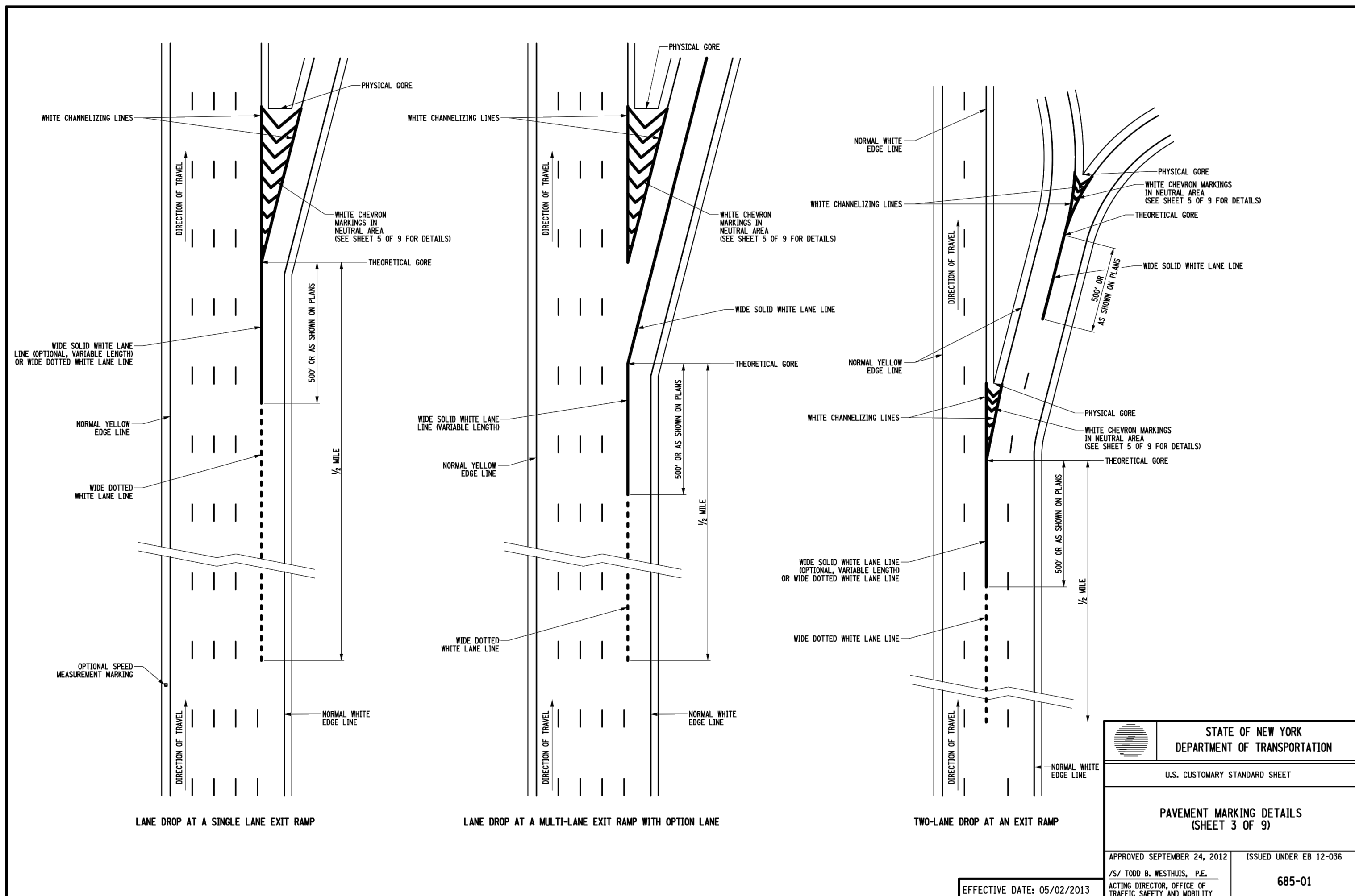
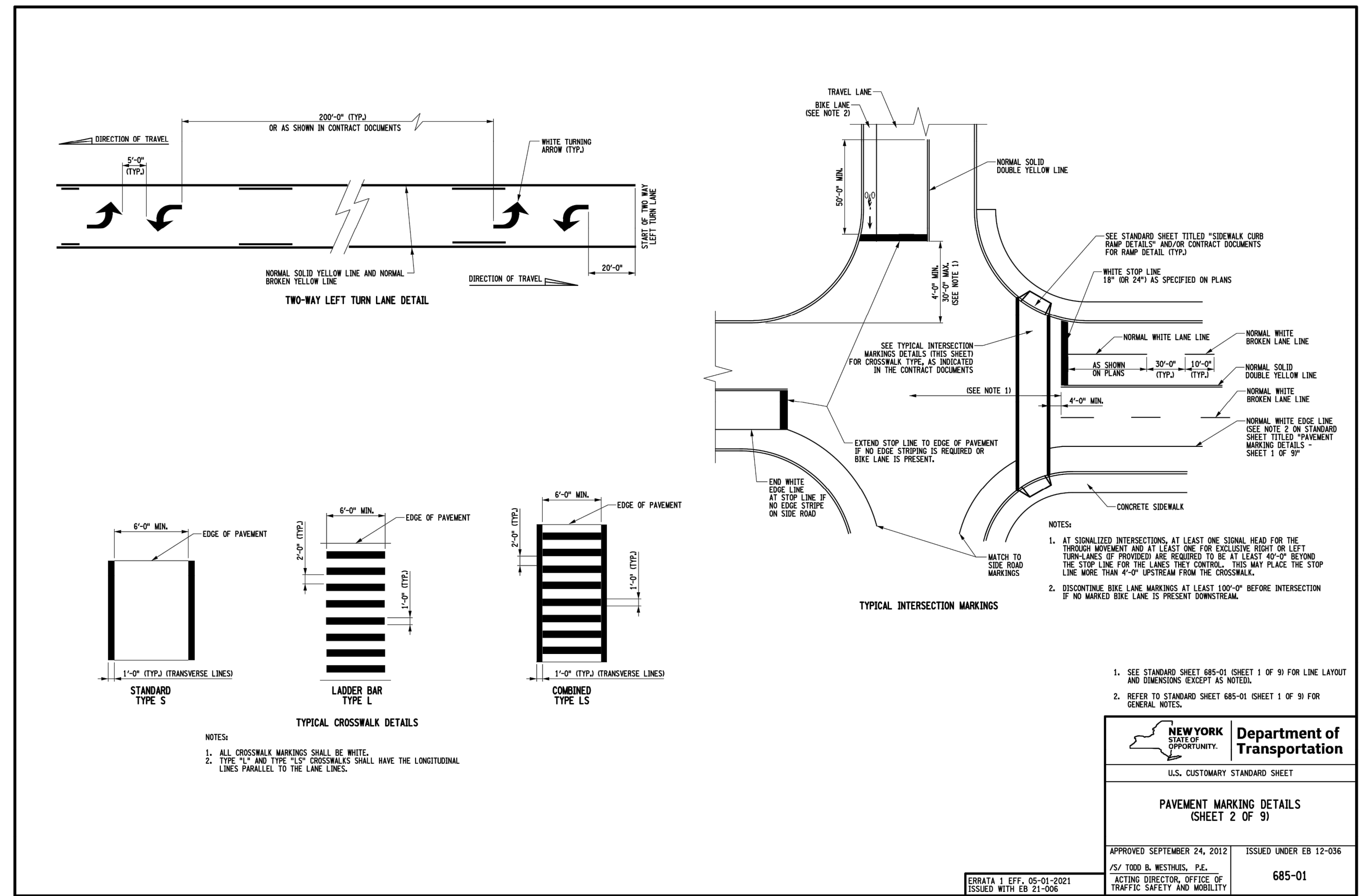
BRATA 1 EIT: 02/01/23  
 ISSUED WITH EB 23-016

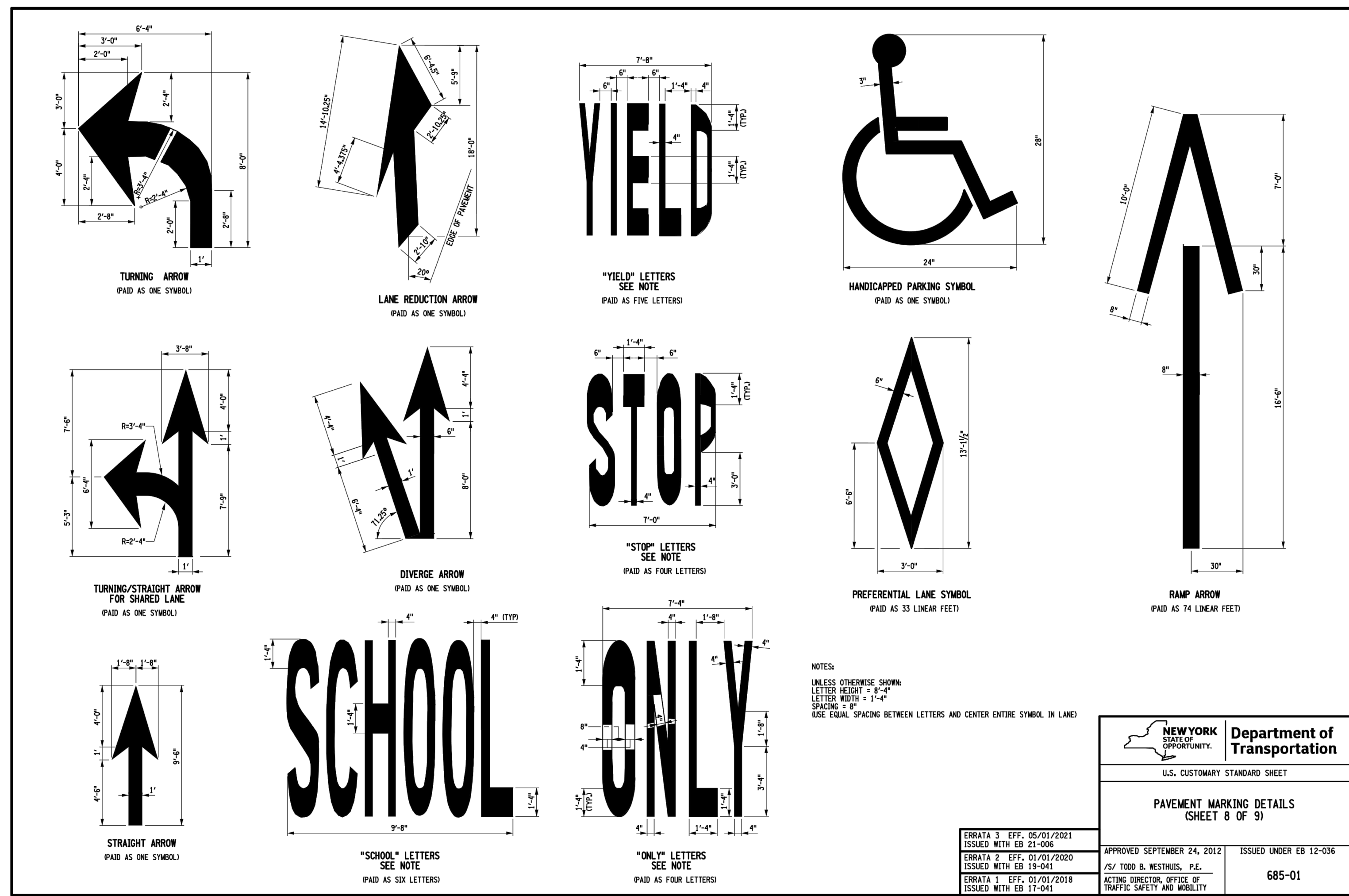
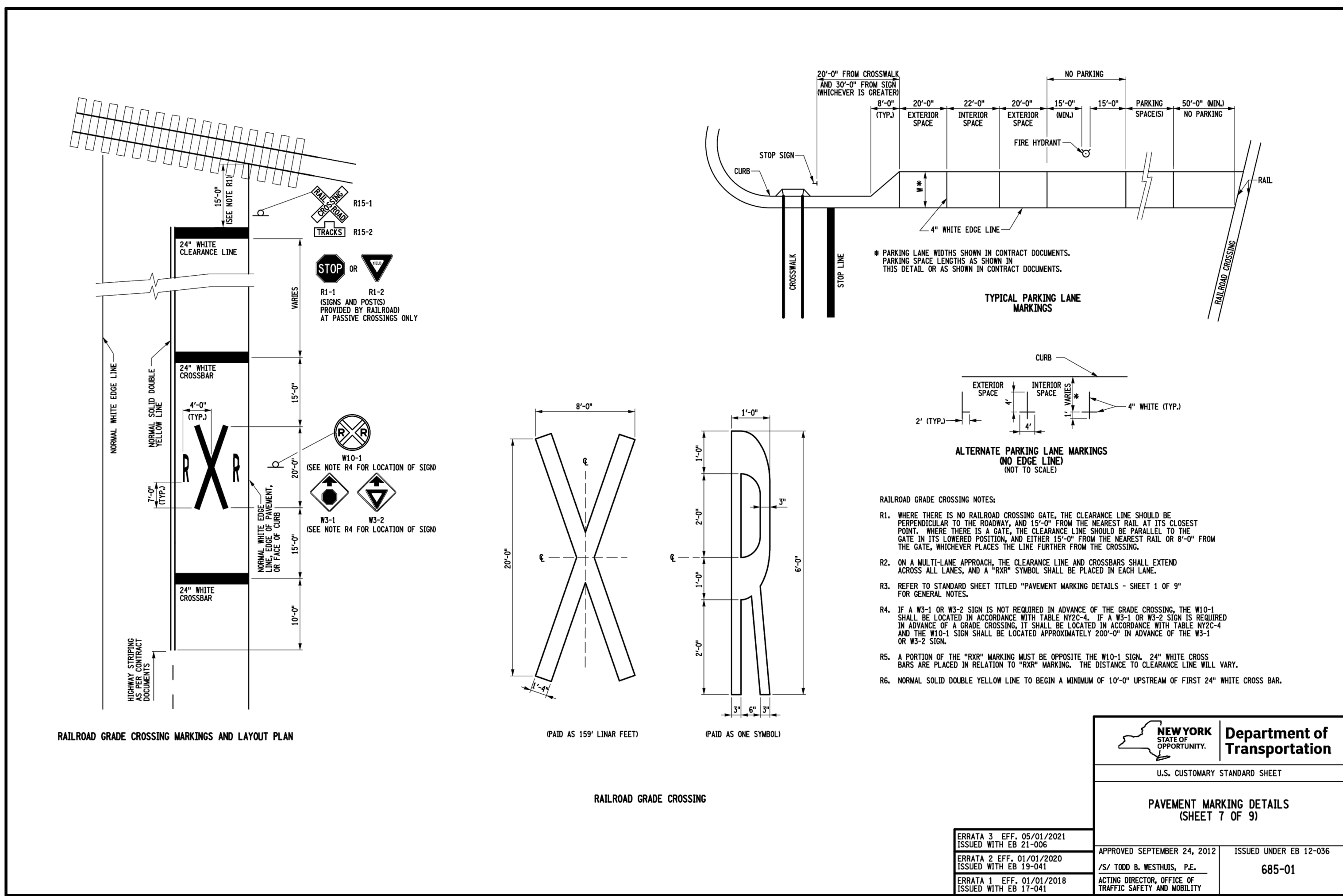
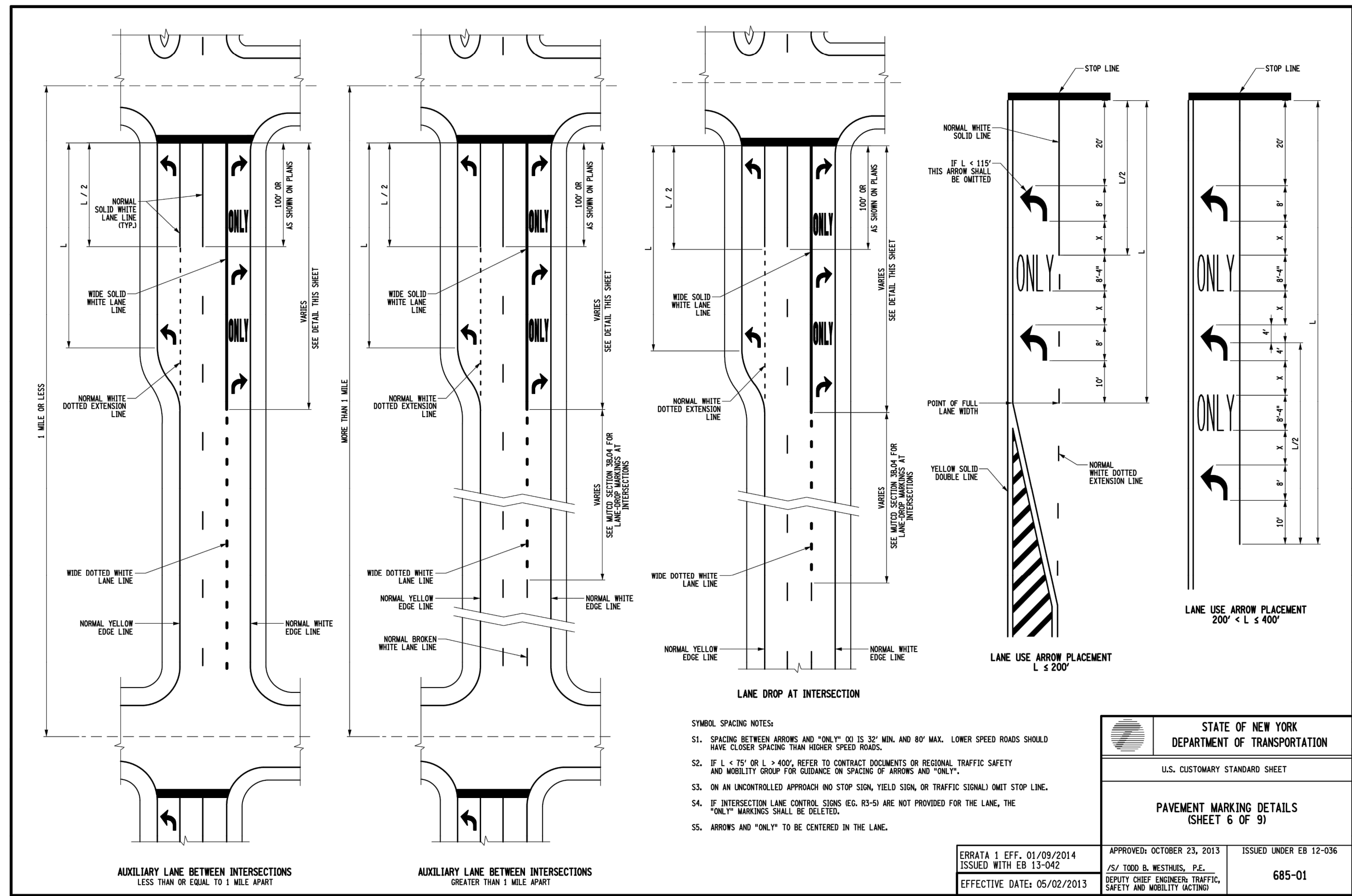
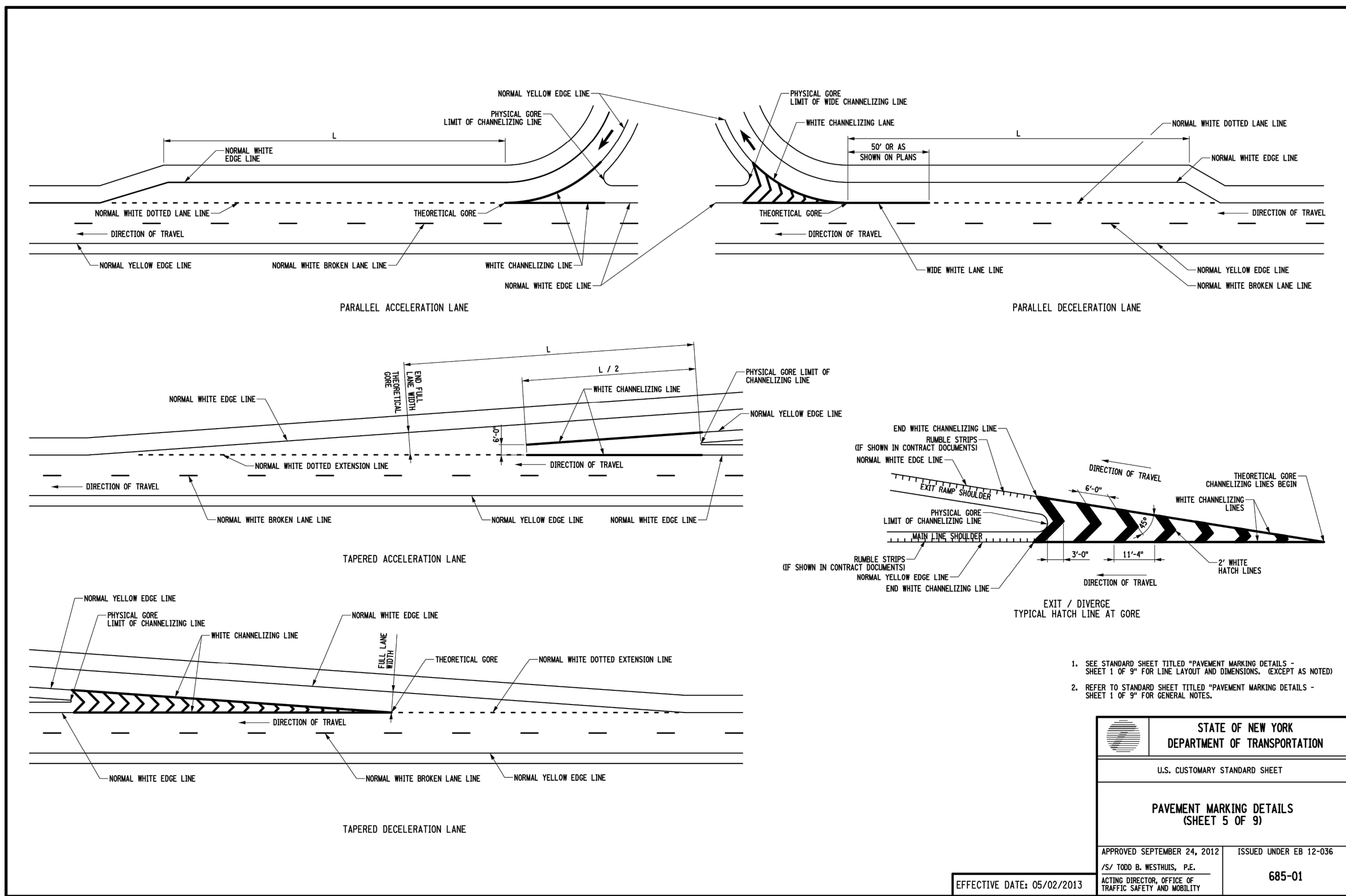


**NEW YORK STATE OF NEW YORK**  
**Department of Transportation**  
U.S. CUSTOMARY STANDARD SHEET

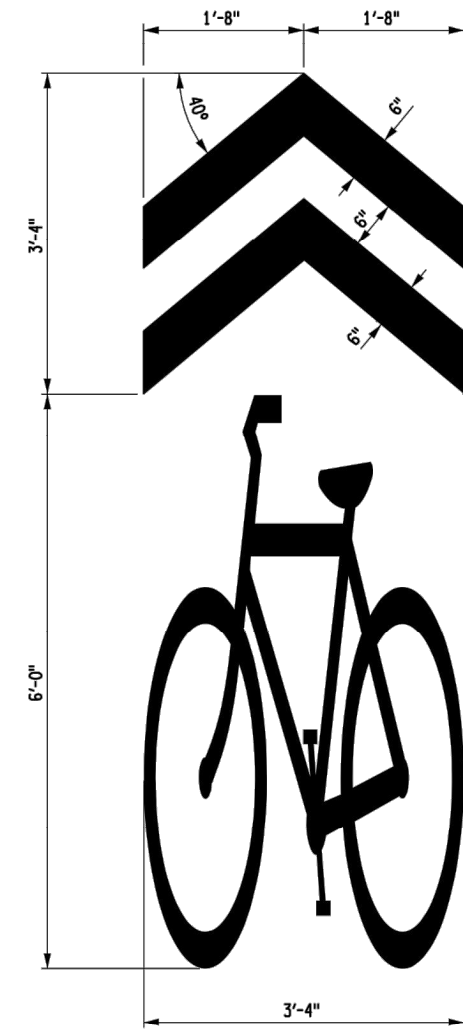
**PAVEMENT MARKING DETAILS (SHEET 1 OF 9)**

APPROVED AUGUST 21, 2018 ISSUED UNDER E1 18-008  
/S/ ROBERT LIMOGES P.E.  
DIRECTOR, OFFICE OF TRAFFIC SAFETY AND MOBILITY 685-01

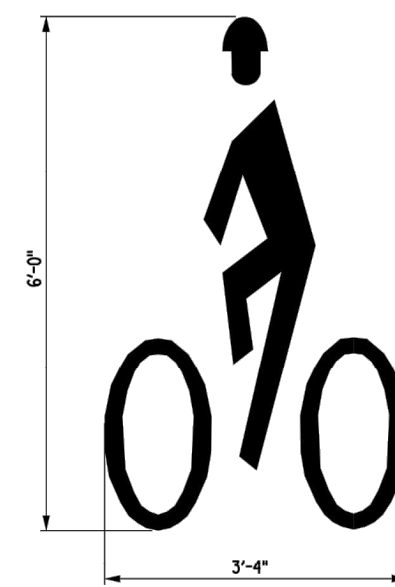




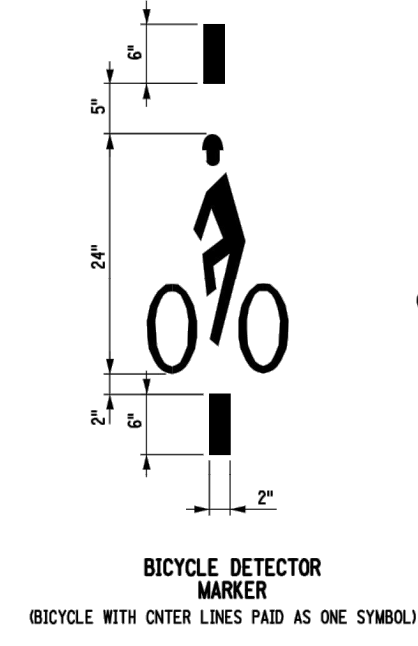
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 USER = P1000



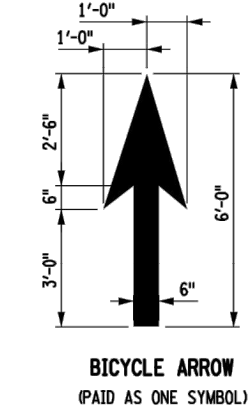
SHARED LANE USE MARKING DETAIL  
 (CHEVRON PAID AS 14 LINEAR FEET)  
 (BICYCLE PAID AS ONE SYMBOL)



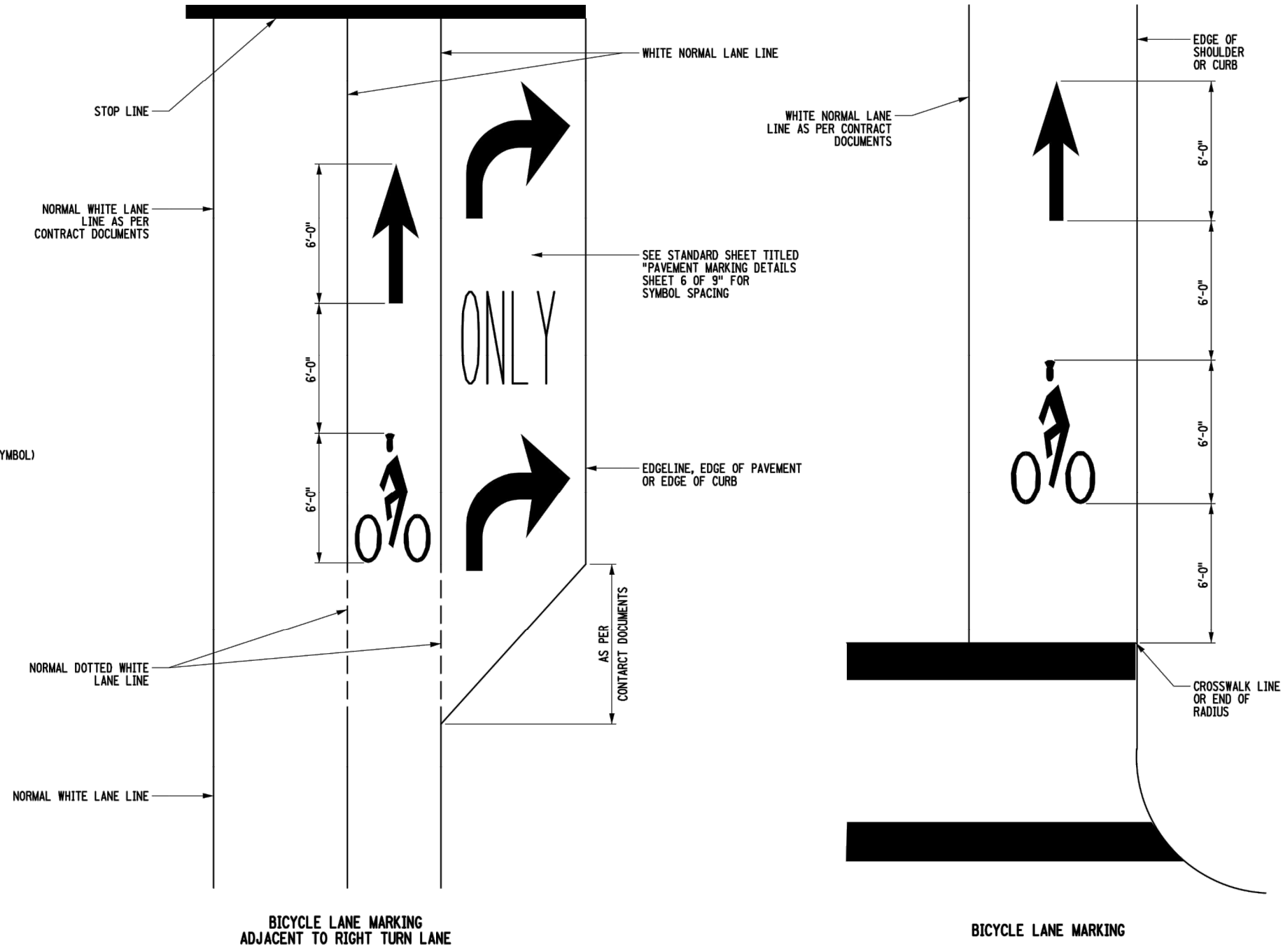
BICYCLE SYMBOL  
 (PAID AS ONE SYMBOL)



BICYCLE DETECTOR MARKER  
 (BICYCLE WITH CENTER LINES PAID AS ONE SYMBOL)



BICYCLE ARROW  
 (PAID AS ONE SYMBOL)



- BICYCLE LANE MARKING NOTES:
- B1. CENTER SYMBOL, IN BICYCLE LANE OR SHOULDER.
  - B2. PLACE BICYCLE LANE ARROW AND SYMBOL DOWNSTREAM OF MAJOR OR SIGNALIZED INTERSECTIONS.
    - PLACE BICYCLE SYMBOL AT 1320'-0" SPACING ALONG BICYCLE LANE OR AS INDICATED IN THE CONTRACT DOCUMENTS.
    - PLACE BICYCLE LANE ARROW AND SYMBOL 200'-0" UPSTREAM OF MAJOR OR SIGNALIZED INTERSECTIONS.
  - B3. MARKINGS AS PER THE DIRECTIONS OF THE REGIONAL OFFICE OF TRAFFIC SAFETY AND MOBILITY GROUP OR AS PER PLAN. SIGNS SHALL BE USED WITH THE BICYCLE LANE.

|                                                   |
|---------------------------------------------------|
| ERRATA 5 EFF. 05-01-2023<br>ISSUED WITH EB 23-007 |
| ERRATA 4 EFF. 09-01-2022<br>ISSUED WITH EB 22-037 |
| ERRATA 3 EFF. 05-01-2021<br>ISSUED WITH EB 21-046 |
| ERRATA 2 EFF. 01-01-2020<br>ISSUED WITH EB 19-041 |
| ERRATA 1 EFF. 01-01-2018<br>ISSUED WITH EB 17-041 |

|                                                                                         |                                                  |
|-----------------------------------------------------------------------------------------|--------------------------------------------------|
| <br>NEW YORK<br>STATE OF<br>OPPORTUNITY                                                 | <b>Department of<br/>         Transportation</b> |
| U.S. CUSTOMARY STANDARD SHEET                                                           |                                                  |
| PAVEMENT MARKING DETAILS<br>(SHEET 9 OF 9)                                              |                                                  |
| APPROVED SEPTEMBER 24, 2012                                                             | ISSUED UNDER EB 12-036                           |
| /s/ TODD B. WESTHUIS, P.E.<br>ACTING DIRECTOR, OFFICE OF<br>TRAFFIC SAFETY AND MOBILITY | 685-01                                           |